

CITY OF FORT LAUDERDALE

DEVELOPMENT REVIEW COMMITTEE



CASE COMMENT REPORT

CASE NO. UDP-S24072



CITY OF FORT LAUDERDALE



CASE INFORMATION

CASE	UDP-S24072
PROJECT NAME	Broward Health Medical Center (BHMC) Medical Office Building and Parking Garage
APPLICATION TYPE	Site Plan Level II
APPROVAL LEVEL	City Commission Approval
REQUEST	Design Deviations for Additional Building Height Greater than 110 Feet in Height for 204,589 Square Feet of Medical Office Use
APPLICANT	North Broward Hospital District
AGENT	Stephanie Toothaker, Esq.
PROPERTY ADDRESS	1601, 1603, 1611, 1627, and 1632 S. Andrews Avenue
ABBREVIATED LEGAL DESCRIPTION	Croissant Park Resub Blk 38 52-8 B Lots 1 To 12, 17 and 18, 21 To 24
ZONING DISTRICT	South Regional Activity Center-South Andrews east (RAC-SAe)
LAND USE	South Regional Activity Center
COMMISSION DISTRICT	4 - Ben Sorensen
NEIGHBORHOOD ASSOCIATION	Poinciana Park Civic Association
SUBMITTAL	December 13, 2024
COMPLETENESS ISSUED	December 19, 2024
EXPIRATION	June 17, 2025 (180 Days)
WAIVER	Not Requested
CASE PLANNER	Adam Schnell Urban Planner III

RESUBMITTAL INFORMATION

- Applicant must provide written responses to all DRC comments contained herein.
- Written responses must specify revisions made to the plans and indicate the sheet.
- Resubmitted plan sets must be accompanied by responses to be accepted.
- Any additional documentation must be provided at time of resubmittal.
- Resubmittals must be conducted through the City's online citizen's portal LauderBuild.
- Questions can be directed to the Case Planner assigned to the case.



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CASE COMMENTS:

Please provide a response to the following:

1. Specify uses and occupancy classification per Chapter 3 of the 2023 FBC.
2. Show provisions for either open or closed interior parking per sections 406.5 and 406.6 of the 2023 FBC.
3. Specify fire-resistance rating requirements based on building separation and construction type per Table 601 and 602 of the 2023 FBC.
4. Provide occupancy loads with compliant life safety egress design per Chapter 10 of the 2023 FBC.
5. Indicate code compliant sprinkler system per Section 903 of the 2023 FBC.
6. Parking facilities that provide valet parking services shall provide at least one passenger loading zone complying with FBC Accessibility Section 503.

GENERAL COMMENTS

The following comments are for informational purposes. Please consider the following prior to submittal for Final DRC:

1. The Florida Building Code shall apply to the construction, alteration, movement, enlargement, replacement, repair, equipment, use and occupancy, location, maintenance, removal and demolition of every building or structure or any appurtenances connected or attached to such buildings or structures.
2. All projects must consider safeguards during the construction process. FBC Chapter 33 delineates various safeguards that may apply during the construction phase. All structures associated with the protection of pedestrians will require a separate permit. A licensed professional must sign and seal the plans and specifications.
3. The City of Fort Lauderdale is a participating municipality in the National Flood Insurance Program (NFIP). The requirements specific to the City of Fort Lauderdale can be found in

Chapter 14 - FLOODPLAIN MANAGEMENT of the Code of Ordinances and accessed at;

- a. https://library.municode.com/fl/fort_lauderdale/codes/code_of_ordinances?nodeId=COOR_CH14FLMA

Please consider the following prior to submittal for Building Permit:

1. On December 31st, 2023 the 8th Edition of the Florida Building Code was adopted. All work described in Section 101.2, of the Broward County Administrative portion of the Florida Building Code, will govern the administration and enforcement of the proposed work. Each building and or structure will require a separate permit. The following websites will assist in the design considerations:
 - b. <https://www.fortlauderdale.gov/government/departments-a-h/development-services/building-services>
 - c. https://floridabuilding.org/bc/bc_default.aspx
 - d. <http://www.broward.org/codeappeals/pages/default.aspx>

General Guidelines Checklist is available upon request.



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CASE COMMENTS:

Please provide updated plans and written response to the following review comments:

1. Meet the City's Adequacy requirements to services provided to the public (fire service, water, wastewater, stormwater, transportation, etc.), per ULDR Section 47-25.2 of the City's Code of Ordinances. Assess potential demands and impacts on City services and prepare a design for each that utilizes existing water, wastewater, stormwater, and transportation infrastructure to adequately serve this project. If the adequate infrastructure is not available, prepare a design that extends/expands the connection to the nearest City system to adequately serve this development.
 - a. Prepare service demand calculations for water & wastewater services and obtain a letter of service availability from the City's Public Works – Engineering Department. Submit water and wastewater capacity availability request form and documents/ plans through the city website. <https://www.fortlauderdale.gov/government/departments-a-h/development-services/building-services/engineering-permits/development-review-committee-service-demand-calculations-for-water-sewer-request-form>
2. Status of existing encumbrances such as easements (including whether public or private) shown on ALTA / NSPS Land Title Survey. Please provide The Certificate executed by the City Engineer evidencing all the conditions from the ordinance ORB 46016 PG 214 have been met.
3. Vacation of any platted Utility Easement would require a separate DRC submittal, DRC staff support, and City Commission approval. Please refer to platted utility easement per PB 52 PG 8.
4. A 'letter of no objection' from each private utility owner that has an interest in all the existing Utility Easements in conflict with the proposed development will also be required; example 10x15 utility easement per ORB 49863 PG 212.
5. Please provide disposition of all the existing utilities (including power poles and street lights) on-site and within the adjacent right of way that may be impacted by the proposed development. Label information on plans (i.e. utility to remain/ be relocated/ removed). Disposition of the existing stormwater drainage system is incomplete both on-site, and off-site (SW 1st Ave).
6. Proposed required on-site improvements shall not be constructed within existing or proposed right of way/ dedications/ easements. Please clearly identify and depict on all the Civil and Landscape plans the Right-of-Way dedication on the West side of S. Andrews Ave. Coordinate with Planning staff the required sidewalk width and provide all the required sidewalk easements accordingly.
 - a. Proposed exterior building doors, loading zone doors, dumpster doors, etc. shall not open into the public Right-of-Way and/or permanently dedicated Right-of-Way Easements and Sidewalk Easements, adjacent sidewalk, ADA accessible path, or drive aisle areas; instead, consider recessing into building to enhance pedestrian safety.
7. Per ULDR Section 47-2.2.Q, show and label all the applicable sight triangles as appropriate for safe and adequate access of drivers and pedestrians, on the Site Plan, Landscape Plans, and Civil Plans: intersections of driveways with streets and alleys (10' measured from intersection point of pavement edges), alleys with alleys (15' measured from intersection point of extended property lines), alleys with streets (15' measured from intersection point of extended property lines), and streets with streets (25' measured from intersection point of extended property lines). Per ULDR Section 47-19.1.E, no accessory use or structure shall be permitted within a sight triangle except as provided in ULDR Section 47-35. Ensure sight triangles are also depicted on adjacent driveways that would be impacted by proposed improvements on this project. Sight triangles located at the intersection of a



local street or driveway with a right-of-way under County, State or Federal jurisdictions, are subject to the sight visibility requirements of those jurisdictions.

- a. Within adjacent City Right-of-Way, staging/storage will not be allowed, construction fence shall not encroach within intersection corner sight triangles, construction fence gates shall not swing into the public Right-of-Way.
8. Passenger loading area is not allowed in the Right of way.
 9. Trash enclosure on site plan.
 - a. Show truck turning movements in and out the proposed dumpster enclosure/ building as applicable.
 - b. Ensure sufficient height clearance is provided within the garage for truck access.
 10. Building Elevations: Show and label existing Right-of-Way, proposed Right-of-Way and/or Easement boundaries, Fee Simple lot boundaries, and horizontal building clearances on all building elevation / section details, as appropriate.
 11. Provide and label typical roadway cross-sections for the proposed development side of S Andrews Ave, SW 1st Ave and SW 17 St: at driveway access points, at on-street parallel parking lanes, and at landscape swale areas (i.e. between travel lane and sidewalk) as applicable. Cross-sections should show existing right of way and proposed right of way and/or easement boundaries.
 12. Label on Site Plan Data Table the required and proposed type of loading zone(s) required, per ULDR Section 47-20.2 Table 2 and Section 47-20.6. Show truck turning template circulation (label typical minimum centerline turning radius) entering and exiting the site as required for the proposed development. Turning geometries and loading zone design shall be in accordance with ULDR Section 47-20.6.
 13. For parking lot layout, annotate on sheets AP-201 to AP-207B:
 - a. Dimension areas including drive aisle widths, and typical parking stall width/depth.
 - b. Per ULDR Section 47-20.11.A, drive aisle width shall be 24' (min.) adjacent to 90-degree angle parking stalls, 18' (min.) adjacent to 60-degree angled parking stalls, 13' (min.) adjacent to 45-degree angled parking stalls, and 12' (min.) adjacent to 30-degree angled parking stalls.
 14. Water and Sewer Plan. Additional comments/ requirements maybe forthcoming from Public Works water and wastewater service availability review.
ADVISORY: Existing 6" water pipe along SW 16 St (vacated Right-of-Way) is an abandoned pipe, per Public Works GIS map.
 15. On-site stormwater retention. The drainage system as per plans does not show overall connections to the existing system serving the remainder of the Surface Water Management license SWM2011-053-0. Please coordinate design with previous SWMS.
 16. Drainage mitigation is required for any impacts within adjacent City of Fort Lauderdale Right-of-Way, such as increased runoff, additional impervious areas, and modification/reduction of existing storage or treatment (i.e. swale areas). Engineer of Record (EOR) shall evaluate the adjacent City roadway system capacity and demonstrate that the proposed improvements will not negatively impact the City's existing drainage system, and provide recommendations in compliance with the City's Comprehensive Plan (i.e. meets or exceeds the 5-year/1-day storm event drainage criteria).



17. Show and label all existing and proposed utilities (utility type, material and size) on civil and landscaping plans for potential conflict (per City Utility Atlas Maps). A min. 5 feet and 10 feet horizontal clearance horizontal separation is required between city utilities infrastructure and proposed small and large trees, respectively (including proposed water and sewer services to the development). Any new trees (located within or adjacent to City Right-of-Way) should be placed with sufficient horizontal and vertical distances (per City, County, and State guidelines) to / from City's public infrastructure, including stormwater assets, to allow for continued Public Works maintenance without obstruction.
18. Proposed trees shall be installed a min. 4 feet behind proposed curbs when adjacent to travel lanes and a min. 6 feet away from adjacent travel lanes when no curb is present.
19. Provide Maintenance Agreement Area Exhibit, which provides a visual representation of the area within the adjacent public Right-of-Way (adjacent to the proposed development) to be maintained in perpetuity by the developer. Label whether the adjacent Right-of-Way is FDOT, BCHCED, or City jurisdiction, as well as label all proposed improvements, including asphalt and other specialty paving, specialty sidewalks, landscaping, irrigation, lighting, curb and gutter etc. that will be maintained by the Applicant throughout the life of the improvements. Perpetual maintenance of newly constructed exfiltration trenches, inlets, etc. within adjacent City Right-of-Way will typically revert back to the City, upon successful inspection/acceptance by Public Works after 1-year warranty period.

For Engineering General Advisory DRC Information, please visit our website at <https://www.fortlauderdale.gov/home/showdocument?id=30249>

Additional comments may be forthcoming at the DRC meeting and once additional/ revised information is provided on plans.



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CASE COMMENTS:

Please provide a response to the following:

1. Building needs to conform to section 403 for high rise.
2. The fire command room must be provided at a street side location with one door leading directly to the outside along South Andrews Ave.

GENERAL COMMENTS

The following comments are for informational purposes before final DRC sign-off.

Florida Fire Prevention Code (FFPC) - NFPA 1 Chapter 15 Fire Department Service Delivery Concurrency Evaluation

15.1 Application.

15.1.1 The AHJ shall be permitted to require a proposed development in the jurisdiction undergo a fire department service delivery concurrency evaluation.

15.1.1.1 Proposed developments that would increase the fire department's service population by less than 1 percent or increase the fire department's total protected building square footage by less than 1 percent shall not be subject to a fire department service delivery concurrency evaluation.

Minimum thresholds that require an evaluation are as follows:

Larger than 1% increase of the City's service population equals any new building with a human capacity of 1856 persons or greater.

Larger than 1% increase in Fire Dept. protected building square footage that equals or is greater than more than 1,132,903 sq. ft.

Note: It only requires one requirement to be a mandatory and must be submitted before a building permit is applied for.



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CASE COMMENTS:

Please provide a response to the following:

1. 2024 FEMA FIRM-Structure is located in a Special Flood Hazard Area, AH 7. Per Sec 14-11(4) of the City of Fort Lauderdale Flood Ordinance No. C-23-46 :

Critical Facilities. New Critical facilities shall be, to the extent possible, located outside the limits of the special flood hazard area. Construction of new critical facilities may be permissible within the special flood hazard area if no feasible alternative site is available. If permitted, the facility shall be elevated or protected to or above the base flood elevation plus three (3) feet or the five hundred-year (0.2 percent chance) flood elevation, whichever is higher. Access routes elevated to or above the base flood elevation shall be provided to the maximum extent possible. The minimum elevation is BFE+3 ft, 10 ft NAVD or the five hundred-year (0.2 percent chance) flood elevation, whichever is higher.

GENERAL COMMENTS

The following comments are for informational purposes.

1. Refer to FEMA Technical Bulletin 3: Requirements for the Design and Certification of Dry Floodproofed Non-Residential and Mixed-Use Buildings.
2. Refer to ASCE 24-14: Flood Resistant Design and Construction
3. Additional comments may follow pending submission of the complete plan set.



Case Number: UDP-S24072

CASE COMMENTS:

Please provide a response to the following.

1. Existing *Cordia sebestena* (#18 & 21) are Small specimen-sized trees as per recent update to the City's Tree Preservation Ordinance. ULDR 47-21.15.G.10: Proposed specimen tree removal requires submittal of an equivalent value evaluation report determined by an ISA Board Certified Master Arborist, ASCA Registered Consulting Arborist or Registered Landscape Architect using a method outlined the Council of Tree & Landscape Appraisers "Guide for Plant Appraisal, 10th edition".
2. Within the SRAC districts, and as per Chapter 2 of the SRAC-SA Illustrations of Design Standards newly planted shade tree street trees are suggested to be a minimum of 20 feet tall; 8 feet spread with 6 feet canopy clearance provided at a ratio of one street tree per thirty feet of street frontage or greater fraction thereof not subtracting ingress and egress dimensions. When overhead utilities exist, required street trees may be small ornamental trees provided at a minimum of a ratio of one street tree per twenty feet frontage or greater fraction thereof not subtracting ingress and egress dimensions. A minimum of 50% of the required street trees must be canopy trees. Small ornamental trees are suggested to be a minimum of 12 feet tall; 6 feet spread with 6 feet canopy clearance. Consider that, in order to maximize the shade for a positive pedestrian experience, the city prefers continuous canopy coverage with 100% canopy trees in these areas. Please verify that specifications meet minimum requirements, and revise if necessary.
3. Within the SRAC districts, and as per Chapter 2 of the SRAC-SA Illustrations of Design Standards, at intersections where streets with shade trees converge, it is encouraged to have palms or ornamental trees at the immediate corners to provide a visual marker. This also helps to frame the street from the perception of the automobile in creating a sense of space. Provide palm or ornamental tree species at the corner of the street intersections in addition to the canopy street trees.
4. A minimum separation of 6 feet is required between the tree trunk and travel lane when curb and gutter DO NOT exist, and a minimum separation of 4 feet is required between the tree trunk and travel lane when curb and gutter DO exist. Illustrate this clearance on landscape plans.
5. The use of structural soil is required in paved sites to provide adequate soil volumes for tree roots under pavements, as per ULDR Section 47-21.13. Structural soil details and specifications can be obtained at <http://www.hort.cornell.edu/uhi/outreach/index.htm#soil> This is to be provided at a minimum of 8' radii of tree trunks, and is to be consistently illustrated and noted on landscape, site and civil plans. This requirement shall apply to SW 5th Street and SW 2nd Avenue tree plantings as well as SW 3rd Avenue.

The structural soil drain is required when percolation rates are less than 4" vertical clearance per hour. Provide documentation of report used to prove this calculation. The drain and connections are to be illustrated on civil plans.

- a) Demonstrate hashing on landscape, site, and civil plans as to the extent of use of the Structural Soil.
 - b) Provide Structural Soil Detail and composition.
6. A suspension modular paving system product may be used in place of CU Structural Soil. Please look into the use of the Green Blue Urban soil cell, Silva cell or like product for this and all future site developments. Provide a detail of product of use for root development under paved areas.
 7. Utilities and site amenities such as walkways, flagpoles, transformers, fire hydrants, sewer and water supply lines, trash enclosures, and similar items located on the site shall not be placed adjacent to, in, or under



- required tree planting areas, as per ULDR Section 47-21.12. Confirm with civil, site and life safety plans that utilities and site amenities are not causing conflicts with proposed landscaping. Where conflicts exist, shift the utility and/or site amenities. Please note and illustrate this on landscape plans.
8. Shade trees must be located a minimum of fifteen feet away from structures, as per ULDR Section 47-21.9. F.1. Please illustrate this clearance on landscape plans, and revise where in conflict.
 9. Light fixtures with an overall height of more than ten feet shall be located a minimum of 15 feet away from shade trees, as per ULDR Section 47-21.12. Please specifically note and illustrate this on landscape plans.
 10. Illustrate and label the horizontal clearance from tree trunk to edge of utility on the landscape plan. Landscaping must provide a minimum horizontal clearance of 5 feet for small trees and palms, and a minimum of 10 feet for large trees and palms from underground utilities. Please revise if in conflict.
 11. Additional comments may be forthcoming after next review of new plans and written comment responses.

GENERAL COMMENTS:

The following comments are for informational purposes. Please consider the following:

1. Note that effective as of November 1, 2024, the City's Tree Preservation Ordinance (ULDR 47-21.15) has been amended by approval of the City Commission. A copy of the amended ordinance shall be made available for use by request in the interim while the City's ULDR found on MuniCode awaits revision publication.
2. A separate sub-permit application for Tree Removal, Relocation, and General Landscaping for site is required at time of master permit submittal. These are Landscape and Tree permit application documents different than the Broward County standard applications. Please do not apply for these at time of DRC submittal.
3. Note that tree removal at time of demolition will not be permitted unless the Master Permit for redevelopment has been submitted for review.
4. Proposed landscaping work in the City's right of way requires engineering approval. This approval requires documents to be submitted for Engineering review. Note that Landscape will not approve permit review without these Engineering documents being submitted.
5. Provide separate Plumbing sub permit application for irrigation. Irrigation plans are required at time of Building permit submittal. Plans are to be in compliance with ULDR 47-21.6. A.11 and 47-21.10. Note that planting areas are to be irrigated on a separate zone than the turf areas so that once the plants are established, that particular zone can be shut off based on the season. The overall goal is to decrease water use through irrigation. Plant material must be grouped together based on watering needs (hydrozone) and turf areas must be limited and/or consolidated to less than 50% of the landscaped area. Illustrate hydrozones on planting plan and include calculations in table.

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CASE COMMENTS:

Please provide a response to the following:

1. Glass doors and windows should be equipped with burglary deterrent features such as track blocks, door pins, or similar devices.
2. All glazing should be impact resistant.
3. Building should be pre-wired for an alarm system.
4. Lighting and landscaping should follow CPTED guidelines.
5. A Video Surveillance System (VSS) should be employed throughout the property with focus on entry/exit points, bay doors, loading zones, parking lot, hallways, offices, and common areas. It should be capable of retrieving an identifiable image of a person.
6. Light reflecting paint should be used in the parking lot to increase visibility and safety.
7. All restricted areas and employee only areas should be access controlled and labelled as such.
8. Office and restroom doors should be lockable from the inside to provide safe shelter in the case of an active threat such as an active killer event.

GENERAL COMMENTS

It is highly recommended that the managing company make arrangement for private security during construction.

Please submit responses in writing prior to DRC sign off.



Case Number: UDP-S244072

CASE COMMENTS:

Please provide a response to the following:

1. Garbage, Recycling and Bulk Trash shall be provided.
2. Recycling reduces the amount of trash your business creates, and it is the best way to reduce monthly waste disposal costs and improve your company's bottom line.
3. Solid Waste Services shall be provided by a Private Contractor licensed by the City.
4. Solid Waste charges shall be collected in monthly lease with Sanitation account for property under one name (Commercial).
5. Service Days shall be: No restriction for Commercial collection. Service may not occur earlier than 7:00 am or later than 10:00 pm within 250 feet of residential.
6. Solid Waste Collection shall be on private property container shall not be placed, stored, or block the public street to perform service (large multifamily and commercial parcels).
7. Provide on the site plan a garbage truck turning radius for City review. Indicate how truck will circulate within property.
8. Solid waste collection shall be from a private loading dock.
9. Solid Waste transport to trash rooms or to primary waste container shall be performed inside building using interior service corridor (Retail, Office, Condo, Hotel).
10. Containers: must comply with 47-19.4
11. Dumpster enclosure: concrete pad, decorative block wall, gates hung independently, protective bollards, secondary pedestrian side entry, high strengthen apron and driveway approach, night light, hot water, hose bib, drain, low circulating ventilation for dampness, weep holes, landscaping, smooth surface walkway to accommodate wheeled containers.
12. Trash Room services will be handled by private collector, or Trash Room services will be done by on site personnel, or Trash Room services will be done by custodial staff.
13. Submit a Solid Waste Management Plan on your letterhead containing the name of project, address, DRC case number, number of units if applicable, and indicate whether it is Pre or Final DRC.
 - *This letter is to be approved and signed off by the Sustainability Division and should be attached to your drawings. Please email an electronic copy to Gwoolweaver@fortlauderdale.gov . The letter should include an analysis of the expected amounts of solid waste and recyclables that will be generated (if different from current capacity), and containers requirements to meet proposed capacity.*
 - *Community Inspections will reference this Solid Waste Plan for sanitation compliance issues at this location.*



GENERAL COMMENTS

The following comments are for informational purposes. Please consider the following prior to submittal for Final DRC:

Please provide specific details of solid waste and recycling collection per building



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CASE COMMENTS:

1. Applicant must fund City's review by consultant and pay a \$4,000 deposit prior to scheduling a methodology meeting after which the study will be prepared, transmitted and reviewed by the City's consultant. Staff and consultant's review concerns shall be adequately resolved prior to gaining authorization for either the Planning & Zoning Board or City Commission hearings. Take into consideration that the review of this study, once the purchase order is created, will take about 4-6 weeks once all documents are received.
2. This proposed development is proposing to remove four (4) metered parking stalls from S Andrews Avenue and five (5) metered parking stalls from SW 1st Avenue. This proposal shall be required to pay a mitigation fee for the permanent displacement of these nine (9) metered parking stalls to the city of Fort Lauderdale's Transportation and Mobility Department. Please provide confirmation from Morgan Dunn mdunn@fortlauderdale.gov from the Transportation and Mobility department that a fee was agreed to.
3. Per city resolution N0.11-147 the applicant shall identify 22 of its parking spaces for the public, and par for the installation of public meters at locations approved by the City Parking Director. Please identify where these 22 parking spaces and meters are located. These 22 metered parking spaces cannot count towards the sites minimum parking requirement. If these spaces are being proposed to be removed, then a parking mitigation fee for the loss of these 22 parking spaces shall be required.
4. Please provide the engineering certificate for the city resolution N0.11-147.
5. Coordinate with Karen Warfel kwarfel@fortlauderdale.gov from the city's Transportation and Mobility department on the approved streetscape improvements on SE/SW 17th Street.
6. On S Andrews Avenue extend the curb and gutter out to edge of travel lane to continue the curb and gutter line from the north to the south.
7. On SW 1st Avenue extend the curb and gutter out to edge of travel lane to continue the curb and gutter line from the north to the south.
8. Remove the proposed private drop off area from SW 1st Avenue. Pick up and drop off for private use must be accommodated on private property.
9. Proposed doors shall not be allowed to swing into the public right of way, recess the doors or use a different door type so that the doors do not open into the public right of way.
10. Include a table showing the proposed land uses, the floor area in square feet for each land use, the parking ratio, the number of parking spaces required by type, and the number of parking spaces proposed by type (standard, compact, handicapped, vertical stacked, tandem, bicycle, loading, etc.). ULDR Sec. 47-20.2. - Parking and loading zone requirements.
11. The parking data table areas and the areas on the plan sheets don't match, please revise.
12. Per section 47-20.2 of the city of Fort Lauderdale ULDR For the purpose of calculating parking spaces, gross floor area shall not include: covered or enclosed parking areas; exterior unenclosed private



balconies; floor space used for mechanical equipment for the building; and, elevator shafts and stairwells at each floor.

13. All parking must be provided in accordance with design and construction standards of the ULDR Section 47-20.11. Show dimensions for: stall width, depth of stall 90 degrees to aisle, aisle width, width of stall parallel to aisle, module width, angle of parking stalls. Properly dimension the parking stall dimensions on the plan sets.
14. Provide any off-site parking agreements this sight may have with other existing or proposed use.
15. Provide a transit shelter for the transit stop on Andrews Avenue.
16. Show inbound and outbound stacking requirements from the property line to the first conflict point according to Section 47-20.5 General design of parking facilities for each proposed driveway. The minimum inbound stacking requirement is five stacking spaces, the minimum outbound stacking requirement is one stacking space. The proposed gate arms conflict with the minimum inbound stacking requirement and must be moved out of the stacking area.
17. For reference one stacking space measurement is 12 feet in width and 22 feet in length.
18. Per ULDR Section 47-2.2.Q, show and label sight triangles as appropriate for safe and adequate access of drivers and pedestrians, on the Site Plan, Landscape Plans, and Civil Plans: intersections of driveways with streets and alleys (10' measured from intersection point of pavement edges), alleys with alleys (15' measured from intersection point of extended property lines), alleys with streets (15' measured from intersection point of extended property lines), and streets with streets (25' measured from intersection point of extended property lines). The request for reduction of sight triangle dimensions is subject to review by the City Engineer on a case-by-case basis. Per ULDR Section 47-19.1.E, no accessory use or structure shall be permitted within a sight triangle except as provided in ULDR Section 47-35.
19. Provide a 10' X 10' sight triangle at the proposed driveways starting at the back of sidewalk and driveway through lanes intersection points, no structures or obstructions shall be placed in this sight triangle. This sight triangle is required to provide clear visibility of the drivers exiting the driveway being able to see the pedestrians walking on the sidewalk against the building.
20. All internal circulation and queuing areas must be designed to accommodate the turning radii of the vehicles that will be using the site. The auto turns vehicular paths to depict the vehicle colliding with a proposed elevator and mounting a non-vehicular use area. Revise the ground floor to prevent these conflicts.
21. Any proposed drainage well, manhole, pull box etc. installed in the sidewalk must be flat, ADA compliant and not impact the effective width of the sidewalk clear path.
22. Ensure all access points, sidewalks, walkways, and curb cuts are unobstructed and ADA accessible with appropriate slopes and detectible warning devices and indicate on the site plan. This includes all access to/from the site entrance. Add the dimension, clearances, and slopes of the walkways.
23. Bicycle parking is strongly encouraged. Consult the APBP Bicycle Parking Guidelines, city of Fort Lauderdale Parking Standards and Broward County End-of-Trip Bicycle Facilities Guide. Look to provide the minimum long term and short-term bicycle parking based on requirements in the guide and include total counts on the site plan data sheet. Bicycle parking needs to be located on the site so that it is accessible to the public.



24. Additional comments may be provided upon further review.

GENERAL COMMENTS

Please address comments below where applicable.

1. The City's Transportation & Mobility Department encourages the use of sustainable materials such as permeable pavement and electric car charge stations and installation of multimodal facilities such as bicycle pump stations and bike lockers.
2. Please note that any work within the City's right-of-way will require an MOT approved by Transportation and Mobility and Engineering for permitting. Any full closures of roadways, alleys, or sidewalks that are over 72 hours will require a Revocable License Agreement (RLA) with the City of Fort Lauderdale.



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CASE COMMENTS:

Please provide a response to the following:

- 1) Pursuant to the State Statute 166.033(1) applications must be deemed approved, approved with conditions, or denied within 180 days, which falls on June 14, 2025, unless a mutually agreed upon time extension is established between the City and the applicant. Failure to meet the applicable timeframe or request an extension will result in the application being denied by the City and the applicant will be required to refile a new application and fees to proceed.
- 2) The site is designated South Regional Activity Center on the City's Future Land Use Map. The proposed use is permitted in this designation. This is not a determination on consistency with Comprehensive Plan Goals, Objectives, and Policies.
- 3) The proposed development application is subject to approval by the City Commission for proposed design deviation requests. The applicant will be required to submit a separate application to be placed on the City Commission agenda and the applicant is responsible for all public notice requirements. Note: The City Clerk's office requires 48 hours' notice prior to a Commission meeting if a computer presentation is planned i.e. Power Point, to be provided on CD or flash drive and a copy submitted to the City Clerk, contact the project planner for more information (954-828-4798).
- 4) In regard to physical, communication, and radar obstructions, the Federal Aviation Administration (FAA) requires a review for interference by the proposed construction. Provide a letter from the FAA indicating that such review has been performed. FAA approval must be obtained prior to Pre-City Commission Sign-off.
- 5) The project does not meet certain South Andrews Avenue Master Plan (SAMP) design intents as outlined in ULDR Section 47-13.30., Table of Dimensional Requirements for the SRAC Districts, Section 47-13.51.B.2., Performance Standards and Criteria for Additional Height Bonus, and South Regional Activity Center- South Andrews Design Standards (SRAC Master Plan).

As outlined in Section 47-13-5, SRAC-SA special regulations, there are two categories of street classification, primary and secondary streets. Primary streets are "characterized by active commercial and retail frontage at the ground floor, taller and more intensive buildings fronting the street, and a consistent streetwall. Primary Streets typically feature a full complement of pedestrian amenities, including wide sidewalks, on street parking, and a well-developed streetscape, which may include open space for public use. Primary Streets are the principal urban streets and are intended to be well used by vehicles and pedestrians and to be the primary transit routes."

The development has a linear frontage of 432 feet along the primary street of South Andrews Avenue and over 100 feet on SE/SW 17th Street. Based on the requirements of the code and the significance of the site's location, changes to the buildings programming and ground level pedestrian realm are recommended and further discussion is needed on the topic of building orientation, garage screening, and first floor activation along the primary corridors.

The massing of the building is located in the middle of the development site, with the entryway located on the secondary street of NW 1st Avenue with 'back of house" uses fronting Andrews Avenue, with only one door functioning as the main point of access. The treatment of the ground level façade is cement plaster, roll up metal doors, utility access and limited amount of windows. The intersection of SE/SW 17th Street and South Andrews Avenue includes a parking garage with a 22-



foot-high blank cement wall, running over 100 feet in length, with no building access, articulation, or change in material. Additionally, the streetscape requirements on both the primary and secondary streets do not meet the master plan requirements.

As provided in the SRAC Master Plan, parking garages are “encouraged to minimize visual exposure of parking by locating active space on the ground floor along the street” and “where structured parking must be exposed to the street, exceptionally creative solutions should be explored”. The current design does not provide respites for pedestrians, outdoor seating, public amenities, or open space. Deviation requests that increase the massing of a structure should provide a superior design and a spatial form that promotes walkable, active, and vibrant streetscapes that support pedestrian interaction with the built environment.

To mitigate these issues the medical office is encouraged to be located on the south portion of the parcel, providing a focal point at the intersection of South Andrews Avenue and SE/SW 17th Street. Placing the parking garage midblock between both office buildings reduces the visual impact of the garage and places the tower at the main focal point of the site. Additionally, it is recommended applicant provide additional floor to ceiling windows and first floor active uses along the southern and eastern elevations, limiting “back of house” uses to the northern and western elevations, and use diversified high-quality material at the ground level to differentiate the pedestrian experience from the garage element. Consider placing the drop-off between both medical buildings to alleviate the right-of-way encroachment, as well as decreasing potential vehicular conflicts by creating a safer drop-off scenario.

Lastly, connectivity between additional medical offices and the Broward Commuter Rail should be considered. As the hospital expands and becomes a medical campus, interconnectivity between medical buildings and access to and from the future rail station will become highly important. The passage between the medical office buildings has an opportunity to become pedestrian-orientated, with wide sidewalks, oblong plazas, with landscaping, and shade trees.

- 6) Provide a narrative stating how the project meets the intent of the DMP with regards to the design deviation requests. Provide required code sections and proposed code deviations in a table format. The table shall provide specificity of the request, identifying on what portion, direction, or floor a deviation is requested, and why.

Staff has commented below under the applicable category and attached the Design Review Team (DRT) comment checklist.

Principles of Street Design

- a. S2 – Lane Width on SW 1st Avenue must be reduce to a 10-foot width per the street design requirements.
- b. S5 – Further discussion with TAM and Engineering is needed on the removal of on street parking spaces.
- c. S9 – Street trees on SW 17th Street will conflict with the existing overhead lines. Underground utilities to reduce conflicts. Overhead lines (if any) should be placed underground. If the lines cannot be placed underground, provide documentation from Florida Power & Light Company indicating such.
- d. S10 – Provide street tree spacing measurements between trees, place palms at corners, shade trees along SW 17th Street, and horizontal clearance measurements between the trees and building face.

Principles of Building Design



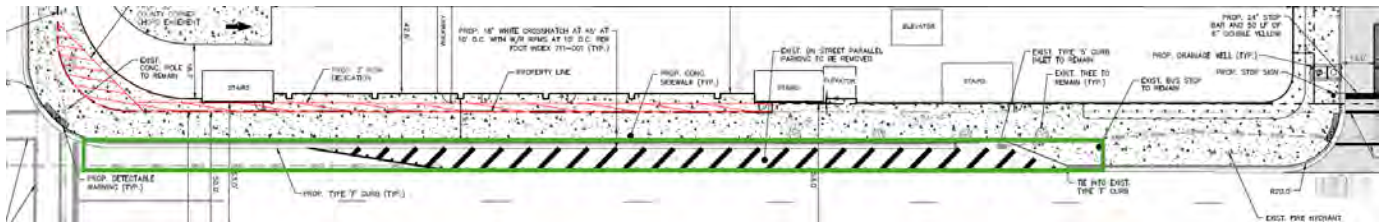
- e. B2 – Parking garage should be positioned midblock. Reorientate building, flipping the office and parking garage to better position the primary visual elements of the building.
 - f. B3 – First floor is not activated, with the primary roadway used as “back of house” uses. Orientate the office building along SW 17th Street and place the entrance of the building along South Andrews Avenue. Remove utility rooms from Andrews and provide additional active space along SW 17th Street and South Andrews Avenue.
 - g. B4 – Parking spaces being removed on South Andrews Avenue shall be converted into a landscaped swale.
 - h. B5 – SW 1st Avenue is currently 4.8 feet; 5 feet is required.
 - i. B8 – The building does not provide variation in the parking deck façade, creating a blank non articulated wall.
 - j. B10 – The maximum floorplate is limited to 20,000 GSF when additional height requests are being sought.
 - k. B12 – Relocation of the office needs to be placed along SW 17th Street.
 - l. B14 – Garage Screening requires additional attention to decrease its visual impact. Placing the garage midblock will further reduce the impact of the garage.
 - m. B15 – Refer to B-16
 - n. B16 – The pedestrian experience needs improvement along SE 17th Street and South Andrews Avenue, increase fenestration and active uses at the pedestrian level.
 - o. B17 – Building does not need meet the requirements. Increase first floor street activation and increase glazing. First floors shall have a minimum height of fifteen (15) feet, and a high percentage of clear glazing (Primary Streets – minimum, 60% and Secondary Streets – minimum 50%).
 - p. B19 – Parking garage stairwells are exposed to the elements. Consider providing a glass enclosure that will aid during rain and wind events, as well as providing an additional architectural improvement.
 - q. B20 – Consider providing shading devices along building perimeter, other than the primary entrance.
 - r. B21 – Providing additional first floor building activation and architectural elements that aid in providing pedestrian orientated environment.
 - s. B25 – Provide an updated photometric plan that includes the garage.
- 7) Consider placement of public art on the development site. See City of Fort Lauderdale’s Comprehensive Plan – Urban Design Element, Goal 2, Objective UD 2.2, Policy UD 2.2.4, which aligns with the City’s goal of encouraging public art features in development projects to enhance the nature of our urban spaces. Placement of public art enhances the overall public realm and vitality of public spaces. It is encouraged that any incorporated art features are clearly visible or easily accessible to the general public from adjacent public property, including sidewalks, streets or other public thoroughfares, and possess functional as well as aesthetic qualities that typically reflect an awareness of a given site, both physically and socially.
- 8) Indicate all utilities (both above and below ground) that would affect the proposed planting or landscape plan. Overhead lines (if any) should be placed underground. If the lines cannot be placed underground, provide documentation from Florida Power & Light Company indicating such.
- 9) Provide the following changes to the Site Plan:
- a. Appropriate placement and compatible building design are important for this site given the context and buildings visibility. Place the medical offices at the southern portion of the parcel and flip the entrance of the building to South Andrews Avenue. Positioning the building at the primary focal point of the intersection promotes a spatial framework that heightens the visual presence of the building, providing direct access to the SE/SW 17th Street crosswalk to the main



hospital building, creating functional connectivity between buildings, increasing pedestrian safety by decreasing midblock crossings. The current design requires pedestrians to traverse the perimeter of the building to cross South Andrews Avenue.

Moreover, as supported by sections B-2, B-3, B-11, and B-12 of the SRAC Master Plan, towers and building massing shall frame the street. Placing the medical office building at the south property line allows the parking garage to be placed between two medical office buildings, helping to shield the visual presence of the exposed garage.

- b. Provide a Survey that is based on a Commitment of Title.
- c. Update the Site Plan to show all sidewalk measurements, building width and length measurements, street tree locations, swale widths, encroachments, and all other sidewalk improvements.
- d. The property requires a unity of title prior to building permit submission.
- e. The development site has an existing office building and parking lot. Provide a master site plan that shows all existing and proposed uses.
- f. The narrative does not match the site plan or site data table. The Master Site Plan shows the medical office building at 188,000 square feet (SF), but the data table on the same sheet shows a total floor area of 186,929 sf. Ensure all narratives and site plan sheets match.
- g. Update the parking calculations using the medical office buildings GSF calculations, also include parking calculations for all existing structures.
- h. Clarify if the parking calculations account for the parking lot spaces being removed and whether the site has any existing offsite parking agreements.
- i. The parking garage calculations are placed on the cover sheet. Relocate all site data information to one sheet and provide totals for the entire building, since the review is not for two different uses. Rather, a medical office building with structured parking.
- j. Update parking calculation statement on the sheet labeled Master Site Plan. A reduction is not being sought. The parking requirements permit decreased parking requirements, but it is not considered a parking reduction.
- k. Per Note A of Section 47-13.30. - Table of Dimensional Requirements for the SRAC Districts, developments seeking heights greater than 110 feet are limited to a floorplate of 20,000 GSF. The site plan shows a floorplate of 23,366 GSF. The alternative design deviation for floorplate request does not apply to properties seeking a height deviation request. Note A has specific floorplate size requirements which are also reflected in the SRAC Master Plan- Section B-9. Moreover, verify the calculations for floorplate size. The Cover Page states there are nine floors at 23,366 GSF, for a total of 204,589 GSF, but the total GSF based on 23,366 GSF is 210,294 GSF.
- l. Provide street sectionals that clearly show measurements from centerline to building face for all improvements, i.e. lane widths, landscaping, hardscape, sidewalks.
- m. Work with Transportation and Mobility on the approved streetscape improvements on SE/SW 17th Street.
- n. The southeast corner does not have a turn lane and can accommodate a larger swale system. Extend the curb and gutter out to edge of travel lane on South Andrews Avenue to continue the curb and gutter line. Based on the available space, an approximate 12-foot-wide sidewalk can be accommodated. The remainder of the streetscape shall be landscaping. Reference image below for additional context. The area highlighted in green shows where the sidewalk and swale is to be expanded.



- o. Per the SRAC Master Plan the maximum travel lane width shall be ten feet. Increase the sidewalk and swale width of SW 1st avenue and remove the drop-off lane that encroaches into the public right-of-way, as well as any sidewalk pinch points. With the removal of the drop-off lane additional trees can be provided. Because no on-street parking is being provided, an approximate 12-foot-wide unobstructed sidewalk can be provided, with a 10-foot-wide landscape swale, and two-foot curb and gutter. Reference image below for additional context. Note: Consider internalizing the drop-off between the existing and proposed building on the vacated portion of SW 16th Street. Reference the image below for additional context.



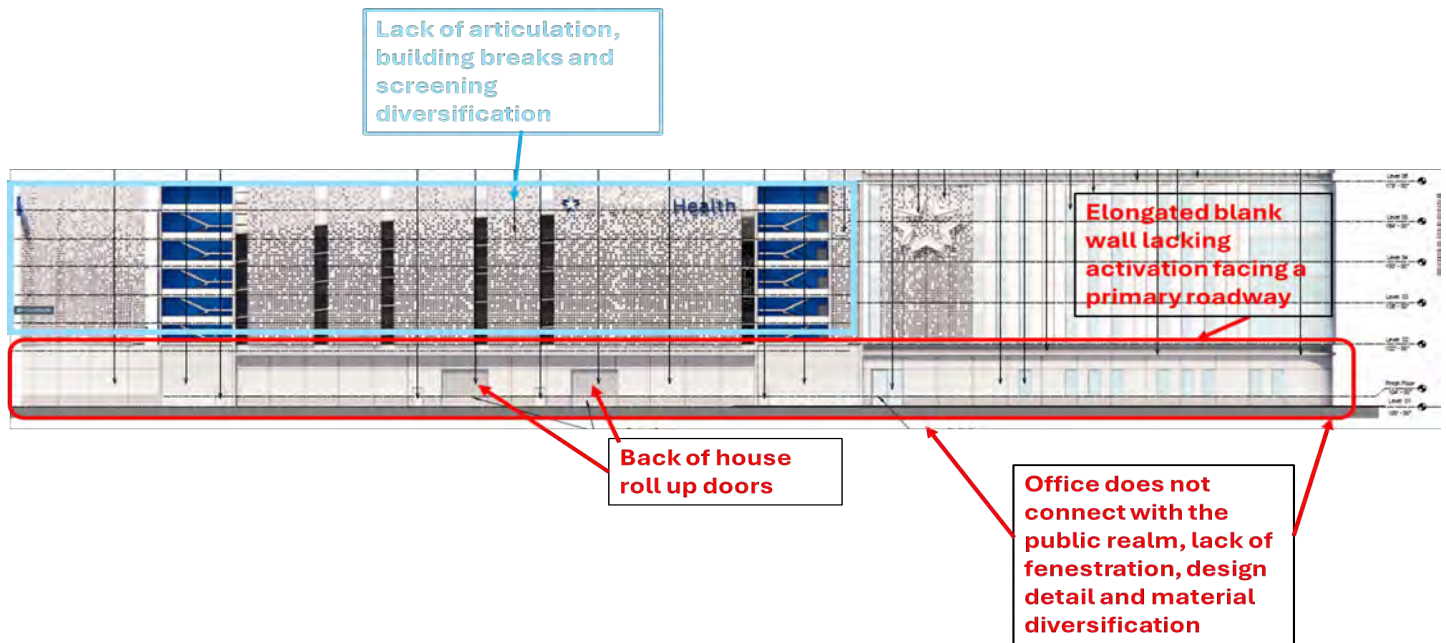
- 10) Provide the following changes on the elevations:
 - a. Place the medical offices at the southern portion of the parcel and flip the entrance of the building to South Andrews Avenue. Reference (Comment 9a) for additional information.
 - b. The parking podium screening needs to incorporate additional architectural elements. The use of perforated metal screening is encouraged, but material diversification over large surface areas is needed to break up the parking garages façade along all frontages.
 - c. The garage stairwells are exposed to the extreme South Florida whether conditions. Enclosing the stairwell with glass or similar material to block weather events increases the stairwells usability and adds an architectural element to the design.
 - d. The massing of the building is located in the middle of the development site, with the entryway located on the secondary street of NW 1st avenue. Andrew Avenues functions as back of house, with one door functioning as the only point of access to office building. Per the SRAC Master Plan, primary entryways shall be placed on primary streets. Reference Comment 10a herein.
 - e. The treatment of the ground level façade on South Andrews Avenue is cement plaster, roll up metal doors, utility access and nine windows and the frontage of SE/SW 17th Street is a parking garage with a 22 foot high blank cement plastered wall, running over 100 feet in length, with no building access, articulation, or change in material. First floors shall have a minimum height of fifteen (15) feet, and a high percentage of clear glazing (Primary Streets – minimum, 60% and Secondary Streets – minimum 50%). The first floor does not meet clear glazing requirements. Provide additional floor to ceiling windows, first floor active uses along the southern and eastern elevations, limiting back of house uses to the northern and western elevations, and use diversified high-quality material at the ground level to differentiate the pedestrian experience from the garage element.
 - f. Elevations need to show distance measurements from property line to building face and distance measurements between the existing onsite buildings.



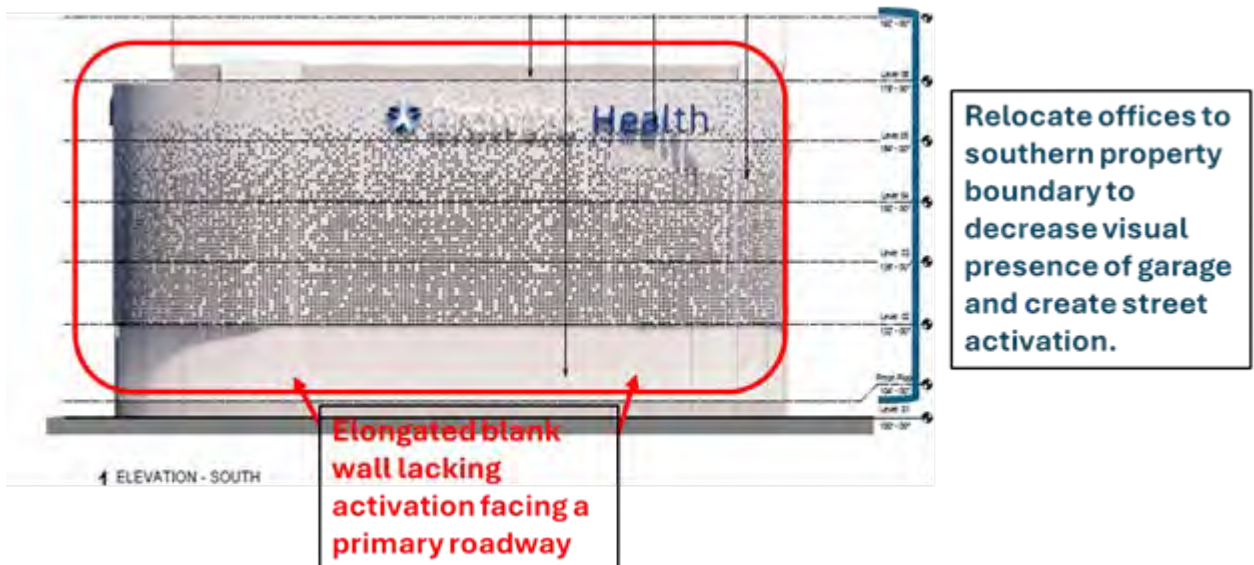
g. Provide tower separation measurements between onsite buildings.

Reference exhibits below that highlight areas of concern. Building design examples have also been provided to offer alternative design solutions.

Andrews Avenue Frontage



SE/SW 17th Street Intersection





Orlando Advent Hospital: Shows a well-integrated medical center, providing interconnected pathways, buildings that match the curvature of the roadway, and separated drop-off areas.





Garage Screening and Corner/ Staircase Design Examples: The examples showcase unique garage screening, corner features that highlight focal points, first floor activation, and changes to texture and material.





Keck Medical Center Arcadia California:

Side View: Activated corner, with internalized parking garage, and midblock entrance.



Front View: Street activation, high levels of fenestration and building break.





Keck Medicine of USC – Ophthalmology – Pasadena: 528 Feet in Length, with activated primary street frontages, and secondary street exposed garage.





Corridor/Connectivity Improvements: An exclusively pedestrian orientated midblock improvement example and pedestrian and roadway example, with the intent of showing activated pedestrian spaces between buildings, plazas and abutting roadways.







- 11) Provide clear image/details of garage screening of openings and light fixture shields and cross-sections. Ensure screening adequately blocks out light and noise pollution.
- 12) Indicate lighting poles and landscape plan and clarify height. Be aware that lighting fixtures greater than 10' in height are used, they shall be located a minimum of 15' away from shade trees (ULDR Section 47-20.14).
- 13) Pursuant to ULDR Section 47-19.2.Z, Accessory Uses, Buildings, and Structures; rooftop mechanical equipment such as air conditioners, compressors, generators, etc. shall be screened with material that matches the material used for the principal structure and shall be at least six (6) inches high above the topmost surface of the roof mounted structures. Provide the following:
 - Spot elevations of the parapet wall and roof as well as mechanical equipment to verify adequate screening;
 - Identify the location of equipment on building elevations by outlining the equipment with dash lines; and
- 14) Pursuant to Section 47-22.4.C.8 provide a master sign plan detailing the following:
 - a. Location and orientation of all proposed signage;
 - b. Dimensions of each proposed sign (height, width, depth, etc.);
 - c. Proposed sign copy; and,
 - d. Proposed color and materials
 - e. Please note any proposed signs will require a separate permit application.
- 15) Provide a construction staging plan which includes anticipated hours of operation on site, debris mitigation plan, and map indicating where crane operations and employee and/or equipment parking and storage will be placed. A revocable license application and a traffic circulation plan may be required if the sidewalk or right-of-way requires to be closed at any time, which should be filed under a separate application and coordinated through the City's Maintenance of Traffic (MOT) process.
- 16) The City's Vision is to support sustainable infrastructure. Consider employing green building practices throughout the project such as, but not limited to; charging stations, tank-less water heaters, rain collection systems, pervious pavement where appropriate, bio-swales, Florida Friendly™ plant materials, solar panels, and green roofs.

GENERAL COMMENTS:

The following comments are for informational purposes.

Please consider the following prior to submittal for Final Development Review Committee:

- 17) An additional follow-up coordination meeting may be required to review project changes necessitated by the Development Review Committee comments. Prior to routing your plans for Final DRC sign-off, please schedule an appointment with the project planner Adam Schnell (954-828-4798) to review project revisions and/or to obtain a signature routing stamp.
- 18) If a temporary construction/sales trailer is needed for this project, provide the details and location of the trailer on a separate site plan, to avoid additional review in the future. Verify details and location with the Building Representative.
- 19) Provide a written response to all Development Review Committee comments.
- 20) Additional comments may be forthcoming at the Development Review Committee meeting.



BACKGROUND INFORMATION: The intent of the Design Review Team (DRT) is to assist applicants by identifying compliance with the applicable master plan design requirements in the form of a checklist. The checklist should be used as a guide in generating formal development applications for site plan submittal. Applicant should provide responses to each item in the DRT checklist upon submittal of formal development applications.

CASE NUMBER:	UDP-DRT24008	<p>NOTES:</p> <ul style="list-style-type: none"> Principles in bold are dimensional standards required by Code (ULDR) Section 47-13.30. Principles marked "yes" / "no" indicate if project meets intent of guideline N/A = Not applicable; more information needed
PROJECT	Broward Health Medical Center	
PROJECT	1627 S Andrews Avenue	
REVIEW DATE:	12/20/2024	
CASE PLANNER:	Adam Schnell	
CONTACT	954828-4798	

INSTRUCTIONS: Mark applicable column with "X". Add comments / design intent under Principle title.

STREET DESIGN STANDARDS: SRAC-SA		YES	NO	N/A	MORE INFORMATION NEEDED
S-1	A fine-grained street grid is maintained, and right-of-ways are vacated only for strategic planning purposes.	X			
S-2	Development above right-of-ways (air rights) does not occur. SW 1st Avenue has been reduced. Revise accordingly.			X	
S-3	Streets have reduced lane widths.		X		
S-4	Traffic calming is utilized rather than barricading streets.			X	
S-5	On-street parking is maximized on all streets. Work with Traffic Engineering on the removal of on street parking spaces.		X		
S-6	Adequate bike lanes are provided where appropriate, subject to planned bicycle network.			X	
S-7	Curb radii are reduced at street intersections to a preferred maximum of 15-feet or a maximum of 20-feet at major arterial roadways.	X			
S-8	County "Corner Cord" requirements are eliminated to the greatest extent possible.			X	



S-9	All utility lines are buried in locations allowing for tree planning and proper root growth. Street trees on SW 17 th Street will conflict with the existing overhead lines. Underground utilities to reduce conflicts.		X		
S-10	Shade trees are maximized on all right-of-ways, located between the sidewalk and the street, with palms or ornamental trees providing a visual marker for intersections (spacing 20-feet for palms/ornamentals & 30-feet for shade trees). Provide street tree spacing measurements, placement of palms at corners, shade trees along SE 17 th Street, and horizontal clearance measurements.		X		
S-11	Landscaping (other than street trees) plays a supporting, rather than dominant, role in the overall street design.			X	
S-12	Numerous and wide curb cuts are avoided to the greatest extent possible. Dropoff is located within City Right-of-Way. Remove drop-off lane from public roadway.		X		
S-13	Drive-thrus are avoided in most cases.			X	

BUILDING DESIGN STANDARDS: SRAC-SA		YES	NO	N/A	MORE
B-1	Surface parking facilities are secondary to the pedestrian public realm experience with vehicular access provided from the secondary street or alley where possible.			X	
B-2	Structured parking design is well integrated into the overall building design. Parking garage should be positioned midblock. Reorientate building, flipping the office and parking garage to better position the primary visual elements of the building.		X		



BUILDING DESIGN STANDARDS: SRAC-SA		YES	NO	N/A	MORE
B-3	<p>To create an interesting, active, street environment, main pedestrian entrances are oriented toward the street.</p> <p>First floor is not activated, with the primary roadway used as back of house. Orientate the entrance of the building to South Andrews Avenue. Remove utility rooms from Andrews and provide additional active space uses along SE 17th Street and South Andrews Avenue.</p>		X		
B-4	<p>Framing the street: Site open space, as required, is aggregated as usable pedestrian-oriented public space instead of leftover "green" perimeter. Courtyards and Plazas that are part of the development site are lined with active uses.</p> <p>Parking spaces being removed on South Andrews Avenue shall be converted into a landscape area.</p>		X		
B-5	<p>Framing the street: Buildings meet the front and corner build-to-lines to maintain a consistent streetwall.</p> <p>SW 1st Avenue is currently 4.8 feet, 5 feet is required.</p>		X		
B-6	<p>Framing the street: Buildings meet the side yard setback to maintain a consistent streetwall.</p>		X		
B-7	<p>Framing the street: Building streetwalls meet minimum and maximum shoulder heights (2 stories or 25-feet minimum & 6 stories or 75-feet maximum).</p>		X		
B-8	<p>Framing the street: Buildings exceeding a maximum streetwall length of 150-feet provide variation in the physical design and articulation of the streetwall.</p> <p>The building does not provide variation in the parking deck façade, creating a blank non articulating wall.</p>	X			
B-9	<p>Buildings do not exceed maximum height dimensions (10 stories/110-feet or 14 stories/150-feet subject to CC review & approval)</p>	X			
B-10	<p>Towers do not exceed minimum stepback dimensions and maximum floorplate area.</p> <p>More Information needed</p>		X		



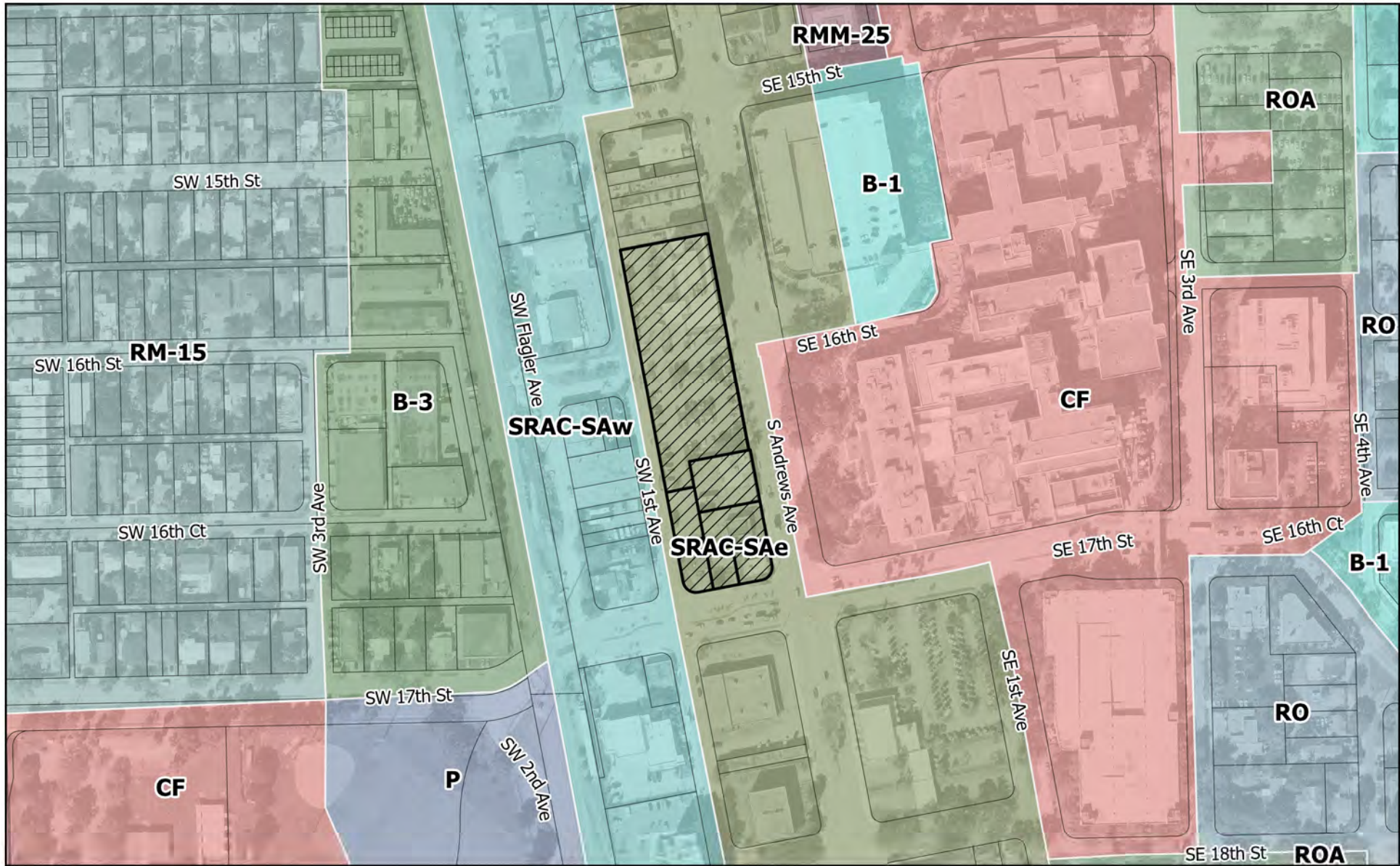
BUILDING DESIGN STANDARDS: SRAC-SA		YES	NO	N/A	MORE
B-11	<p>Where buildings with towers are located with frontages on multiple streets, the towers are oriented towards the "Primary Street".</p> <p>Reference comment B-3</p>		X		
B-12	<p>Towers contribute to the overall skyline composition.</p> <p>Relocation of the office use needs to be placed along SE 17th Street.</p>		X		
B-13	<p>Original and self-confident design: A range of architectural styles exist, each having a strong identity, and striving for the highest quality expression of its chosen architectural style.</p>				X
B-14	<p>Buildings are of high-quality design and construction with an emphasis on durable materials, well thought-out details and careful workmanship.</p> <p>Garage Screening requires additional attention to decrease its visual impact. Placing the garage midblock will further reduce the impact of the garage.</p>		X		
B-15	<p>Buildings are site responsive, reflect local character, and have architectural features and patterns that provide visual interest from the perspective of the pedestrian.</p> <p>Refer to B-16</p>		X		
B-16	<p>Creative façade composition: A rich layering of architectural elements are provided throughout the building, with special attention to details below the shoulder level.</p> <p>The pedestrian experience is nonexistent, increase fenestration, with large blank walls, at the lower pedestrian level.</p>		X		
B-17	<p>The first floor of nonresidential buildings are flush with the adjacent sidewalk, have a minimum height of fifteen (15)</p>		X		



BUILDING DESIGN STANDARDS: SRAC-SA		YES	NO	N/A	MORE
	<p>feet, and a high percentage of clear glazing (Primary Streets – minimum, 60% and Secondary Streets – minimum 50%).</p> <p>Building does not need meet the requirements. Increase first floor street activation and increase glazing.</p>				
B-18	Buildings with historic value are preserved and utilized for adaptive re-use.			X	
B-19	<p>Architecture responds to the unique nature of the South Florida environment.</p> <p>Parking garage stairwells are exposed to the elements. Consider providing a glass enclosure that will aid during rain and wind events, as well as providing an additional architectural improvement.</p>		X		
B-20	<p>Pedestrian shading devices, of various types, are provided along the façade of buildings.</p> <p>Consider providing shading devices along building perimeter, other than the primary entrance.</p>		X		
B-21	<p>Active and ‘extroverted’ ground floors with retail are located in strategic location.</p> <p>Providing additional first floor building activation and architectural elements that aid in providing pedestrian orientated environment.</p>		X		
B-22	In residential buildings, ground floor units have individual entrances.			X	
B-23	Balconies and bay windows animate residential building façades.			X	
B-24	The ‘Fifth Façade’ of a building is treated as part of the total design.	X			
B-25	Lighting is utilized to enhance safety without contributing to excessive light pollution or glare.		X		



BUILDING DESIGN STANDARDS: SRAC-SA		YES	NO	N/A	MORE
	Provide an updated photometric plan that includes the garage.				
B-26	Noise pollution as a result of building design is mitigated.			X	



UDP-S24072

Legend

-  Fort Lauderdale Municipal Boundary Line
-  Subject Site

