



CITY OF FORT LAUDERDALE

DRAFT

**AVIATION ADVISORY BOARD MEETING MINUTES
FORT LAUDERDALE EXECUTIVE AIRPORT
RED TAILS CONFERENCE ROOM
6000 NW 21 AVENUE, FORT LAUDERDALE, FLORIDA
THURSDAY, JUNE 27, 2024 – 1:30 P.M.**

Board Members	Attendance	Cumulative Attendance 7/2023 - 6/2024	
		Present	Absent
Mark Volchek, Chair	P	6	1
William Gilbert, Vice Chair [via Zoom]	P	6	1
David Ash	P	5	2
Kelly Gordon	P	5	2
Dr. Jeff Johnson	P	6	1
Dr. Ed Kwoka	P	6	1
Robert Laughlin	P	7	0
Kevin Macelhaney	P	6	1
Valerie Vitale	P	6	1
John Vratsinas	P	6	1
Non-Voting			
Commissioner Marlon Bolton	A	4	3
Jeff Helyer, City of Oakland Park	P	6	1

Airport/City Staff

Rufus A. James, Airport Director
Carlton Harrison, Assistant Airport Director
Khant Myat, Project Manager II
Chelsea Jones Finley, Airport Programs Manager I
Angelia Basto, Senior Financial Administrator
Miguel Laca, Financial Administrator
Linda Blanco, Senior Administrative Assistant
Ronald Aristide, Airport Maintenance Technician
LaTorrance Garrett, Intern

Others

Travis Werth, JM Family Enterprises
Debbie Puia, Lofts of Palm Aire
Patrick Madden, Coral Ridge Isles Civic Association
Barbra Stern
Marshall Myles, FXE Jet Center
Alexander Rivera, FXE Jet Center
Tony Sherbert, Sheltair
Johanna Carter, Sheltair
Jon Tonko, Banyan Air Service
Ignacio Martinez, W Aviation

CALL TO ORDER

Chair Volchek called the meeting to order at 1:33 p.m.

ROLL CALL

Roll was called and a quorum was determined to be present.

Motion made by Mr. Vratsinas, seconded by Dr. Kwoka, to allow Mr. Gilbert to attend the meeting via Zoom. Motion passed unanimously.

APPROVAL OF MINUTES

- **April 25, 2024**

Motion made by Mr. Laughlin, seconded by Mr. Ash, to approve the minutes of the April 25, 2024, meeting as presented. Motion passed unanimously.

ELECTIONS

Mr. Macelhaney, seconded by Mr. Gilbert, nominated Mr. Volchek for Chair. Mr. Laughlin nominated Dr. Kwoka but there was no second. In a roll call vote, Mr. Volchek was elected unanimously.

Mr. Ash, seconded by Mr. Laughlin, nominated Dr. Kwoka for Vice Chair. Chair Volchek, seconded by Mr. Vratsinas, nominated Mr. Gilbert for Vice Chair. In a roll call vote, Mr. Gilbert was elected with 6 votes and Dr. Kwoka received 3 votes.

Dr. Kwoka stated his support for Mr. Gilbert.

VOTING ITEMS

1. Minimum Annual Lease Rates and Real Estate Brokerage Commission

Mr. James provided a presentation and recommended a minimum aviation lease rate of \$0.65 per square foot, with annual CPI adjustments, periodic fair market adjustments, a maximum term of 30 years, and no brokerage commission paid on leases of Airport property. He also recommended a minimum lease rate for non-aviation property consistent with FAA guidelines, with annual CPI adjustments, periodic fair market adjustment escalations, and a maximum term of 50 years; based on MAI appraisal established within six months of the effective date of the lease agreement or such period as approved by the FAA.

Motion made by Mr. Laughlin, seconded by Ms. Vitale, to approve staff's recommendation regarding Minimum Annual Lease Rates and Real Estate Brokerage Commission. Motion passed 8-1 with Mr. Vratsinas opposed.

2. Transition from Aviation Gas to Aircraft Unleaded Fuel Initiative

Mr. James provided a presentation and recommended the continued implementation of sustainable practices at the Fort Lauderdale Executive Airport. This will support the Eliminate Aviation Gasoline Lead Emissions initiative and promote programs for a safe transition to aircraft unleaded fuel for piston-engine aircraft by 2030.

Motion made by Mr. Vratsinas, seconded by Dr. Kwoka, to approve the staff recommendation. Motion passed unanimously.

UPDATE ITEMS

A. Noise Compatibility Program

Ms. Jones Finley reported that in April, there were four aircraft exceeding the recommended limit of 80 decibels at Noise Monitor 2 on Runway 9, one of which had generated three noise reports from the community. There was a total of 57 noise reports from eight households in April. In May, there were two aircraft exceeding the recommended limit of 80 decibels at Noise Monitor 2 on Runway 9, one of which had generated two noise reports from the community. In May, there were a total of 38 noise reports from 11 households.

Ms. Jones Finley reported Noise Monitor 1 was up and had electricity. It worked for one day but there was a technical issue that they anticipated would be resolved in a few weeks.

Dr. Kwoka said he spoke with Commissioner Herbst and Mr. James regarding appointing a representative from The Lofts of Palm Aire to the Board. He recalled he made a statement at a previous meeting that offended residents and said his remark was “based purely on aviation and lacked consideration for the fact that these are their homes.”

B. Development and Construction

Runway 9 Run-up Area Relocation and Southend Taxiways Project

Mr. Harrison said they were now in Phase 4 and paving the areas between Taxiway Alpha 1 and Taxiway Alpha 3. Next week they would begin Phases 5 and 6 which would close Taxiway Echo between Echo 2 and Echo 5 [one at a time].

Taxiway 31 Bypass Project

Mr. Harrison stated this project began on April 8 and all work was being done at night. The Taxiway should be open in the next 30 days for regular use.

Operational data was included in the Board’s packet.

C. Arrearages

Mr. James reported there were no rent or fuel flowage arrearages to report.

D. Communication to the City Commission

None

E. Other Items

Mr. Laughlin asked about the JM Family Automotive lease agreement they had discussed in February and Mr. James said they were still in discussions with W Aviation.

I. Runway 9-27 Rehabilitation Project Update

Mr. Myat reported the construction contract should be awarded in September and the mobilization is expected to begin shortly thereafter. The primary scope of the project is to mill and resurface Runway 9-27, electrical work, runway grooving, and work at the intersections to match the new runway elevation. The construction would require the full closure of Runway 9-27 and partial closure of Runway 13-31.

He referred to the closure schedule:

Phase 1 to 3:

- Runway 9-27 -Closed for 24 hours for ten consecutive days.
- Runway 9-27 and Runway 13-31 – Closed for 24 hours for three consecutive days. This closure is within the ten-day closure.

Phase 4 to 6:

- Runway 9-27 Closed nightly for 51 days; 30 days after completion of Phase 3.
- Runway 9-27 and Runway 13-31 - Closed nightly for two consecutive weekends. This closure is within the 51-day closure.

Mr. James said the Airport had a 10-day closure of the runway in 2004 and it is now at the point where it required repaving. Dr. Kwoka inquired as to whether this would coincide with the construction of the 1,000-foot extension and Mr. James replied that it would not. They would schedule the closures not to coincide with the busy season even if this meant postponing the work.

Dr. Kwoka was adamant that this had been done in the last five to six years, but Mr. James said this was not the case.

Mr. James announced that the Runway Safety Action Team would be visiting on July 16.

Mr. James wished everyone a happy Independence Day.

Mr. James introduced summer high school intern, LaTorrance Garrett from Dillard High School.

II. Public Comments

Patrick Madden, Airport and Noise Liaison for Coral Ridge Isles Civic Association, thanked Ms. Permanan for treating them as stakeholders in the process of getting their noise monitor working. He said his neighborhood association would appreciate a seat on the Board. Mr. James pointed out that Mr. Laughlin and Dr. Johnson represent District 1 on the Board so they can reach out to them if they have any concerns.

ADJOURNMENT

There being no further business to come before the Board at this time, the meeting was adjourned at 2:42 p.m.

NEXT SCHEDULED MEETING DATE: Thursday, July 25, 2024, at 1:30 P.M.

Any written public comments made 48 hours prior to the meeting regarding items discussed during the proceedings have been attached hereto.

VOTING ITEM 1

DATE: August 22, 2024
TO: Aviation Advisory Board
VIA: Rufus James, Director
BY: Khant Myat, P.E., Airport Engineer
SUBJECT: Executive Airport – Runway 9-27 Rehabilitation Project - Federal Aviation Administration Grant Acceptance and Bid Award

The Runway 9-27 Pavement Rehabilitation project is required as part of the Airport’s approved Airport Layout Plan (ALP) to improve the current pavement condition of Runway 9-27 and taxiway intersections. Runway 9-27 is currently in poor condition with an area weighted Pavement Condition Index (PCI) value of 47 per the Florida Department of Transportation (FDOT) Airport Pavement Evaluation Report of Fort Lauderdale Executive Airport (FXE). The 2023 FDOT Pavement Evaluation Report recommended Runway 9-27 be milled and overlaid with P-401 asphalt.

The existing asphalt on Runway 9-27 underwent its last major rehabilitation in 2004. The total area for pavement rehabilitation is approximately 650,000 square feet which includes the runway and connector taxiways up to the runway hold position markings. Additionally, the scope of the construction includes milling, paving, electrical installation, pavement markings, geotechnical work and construction survey.

On June 21, 2024, the City’s Procurement Services Division issued an Invitation to Bid (ITB) for the Runway 9-27 Pavement Rehabilitation Project. The City received the following three (3) bids in response to the solicitation, which was opened on July 23, 2024:

General Asphalt Co. LLC	\$ 9,772,352.51
Ranger Construction, LLC.	\$11,214,644.58
OHLA USA, Inc.	\$11,545,001.75

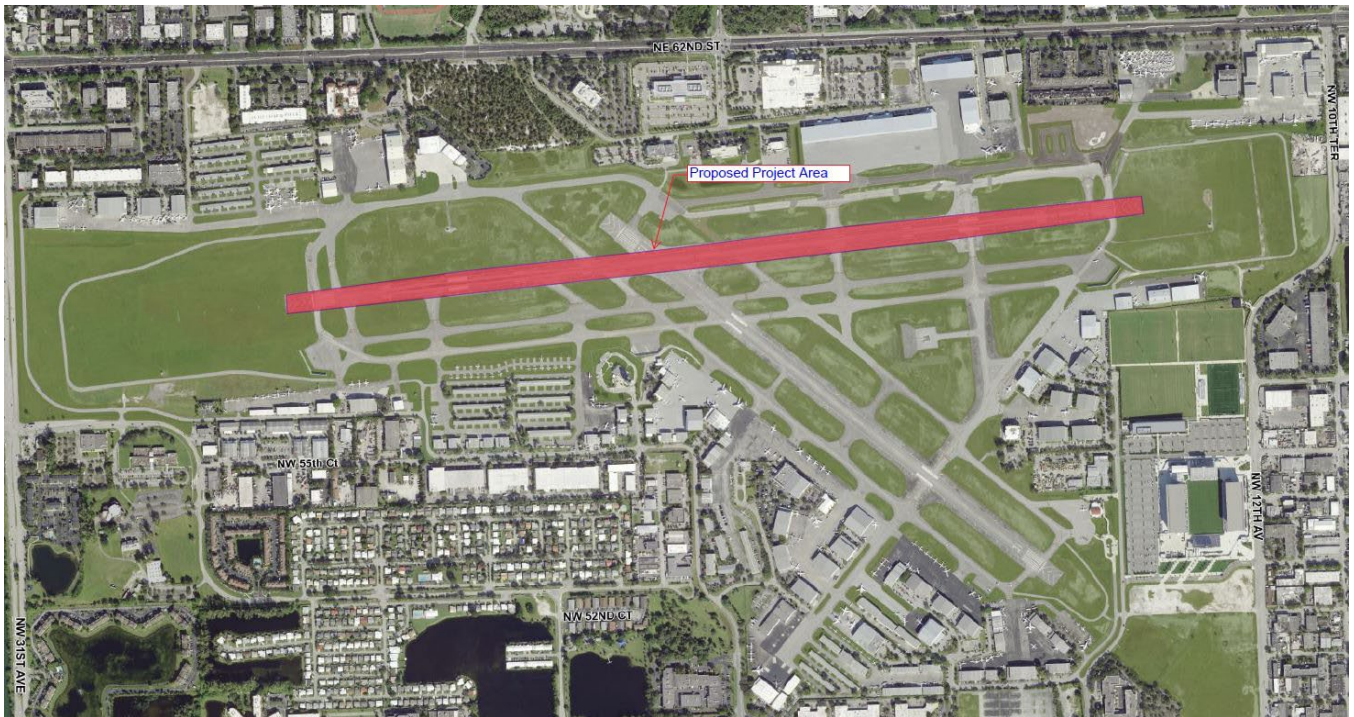
General Asphalt Company (GAC) is currently deemed as apparent lowest, responsible and responsive bidder. City Procurement Services is reviewing the references and submittal documents from GAC. General Asphalt Company has successfully completed projects at FXE including Taxiways Hotel and Quebec Intersection Improvement project, Taxiway Sierra Pavement Rehabilitation project, and the Taxiway Foxtrot Relocation project. All the projects were completed on time and within budget

The City is also anticipating receiving an Airport Improvement Program (AIP) grant in the approximate amount of \$8,800,000 or up to 90% of the construction cost from Federal Aviation Administration (FAA) in September 2024. The Florida Department of Transportation has already provided a grant in the amount to \$440,771. The remaining construction costs will be covered by the Airport Project Fund.


Staff Recommendation:

1. Staff recommends award of the contract to the low-responsive bidder, General Asphalt Company, LLC., in the amount of \$9,772,352.51.
2. Staff recommends acceptance of an FAA AIP Grant for approximately 90% federal funding participation in the above project, up to an amount of approximately \$8,800,000.

VOTING ITEM 1 EXHIBIT 1



UPDATE ITEM A

DATE: August 22, 2024
TO: Aviation Advisory Board 
FROM: Rufus A. James, Airport Director
VIA: Chelsea Jones Finley, Airport Programs Manager
BY: Krystal Permanan, Airport Business Assistance Administrator
SUBJECT: Executive Airport – Noise Compatibility Program

This update comprises the reports for June and July 2024.

In June 2024, Fort Lauderdale Executive Airport (FXE) generated 1071 jet departures from Runway 9. Noise Monitor #2 recorded one aircraft that registered at the 80 decibels (dB) threshold; however, it did not result in any community noise complaints. The noise event was associated with the departure of a Bombardier Challenger 300 aircraft. A total of 59 noise reports were received in June 2024.

In July 2024, FXE generated 919 jet departures from Runway 9. Noise Monitor #2 reported no aircraft registering at 80dB or above. A total of 34 community noise reports were received in July 2024.

Staff will continue responding to residents' concerns and analyzing aircraft operations to ensure safe and efficient operations at FXE.



COOPERATIVE NOISE ABATEMENT EFFORT

Month of June, 2024

August 22, 2024

Jet Departures and Noise	May-24	Jun-24
Total Departures	1182	1071
Runway 27 Jet Departures	274	262
Runway 9 Jet Departures	895	802
Runway 13/31 Jet Departures	13	7
Runway 9 Jet Departures, I-95 Turns	582	503
Runway 9 Jet Departures %, I-95 Turns	65%	63%
Jet over 80dB at Monitor #1	*	27
Jet over 80dB at Monitor #2	2	1

Noise Reports	May-24	Jun-24
Number of Households - FXE Noise Only	11	10
Number of Reports - Jets	16	25
Number of Reports - Propellers Single-Engine	10	21
Number of Reports - Propellers Multi-Engine	9	11
Number of Reports - Helicopters	1	0
Number of Reports - Unknown AC Category	2	2
Number of Reports - Unmatched	0	0
Number of Reports - DT1	0	0
Total Number of Noise Reports for FXE ¹	38	59

¹ The Total Number of Noise Reports for FXE excludes Number of Households and Unmatched Reports.

Noise Reports By Quadrant





COOPERATIVE NOISE ABATEMENT EFFORT

Month of July, 2024

August 22, 2024

Jet Departures and Noise	Jun-24	Jul-24
Total Departures	1071	919
Runway 27 Jet Departures	262	75
Runway 9 Jet Departures	802	840
Runway 13/31 Jet Departures	7	4
Runway 9 Jet Departures, I-95 Turns	503	539
Runway 9 Jet Departures %, I-95 Turns	63%	64%
Jet over 80dB at Monitor #1	27	16
Jet over 80dB at Monitor #2	1	0

Noise Reports	Jun-24	Jul-24
Number of Households - FXE Noise Only	10	8
Number of Reports - Jets	25	12
Number of Reports - Propellers Single-Engine	21	13
Number of Reports - Propellers Multi-Engine	11	8
Number of Reports - Helicopters	0	0
Number of Reports - Unknown AC Category	2	1
Number of Reports - Unmatched	0	0
Number of Reports - DT1	0	0
Total Number of Noise Reports for FXE ¹	59	34

¹ The Total Number of Noise Reports for FXE excludes Number of Households and Unmatched Reports.

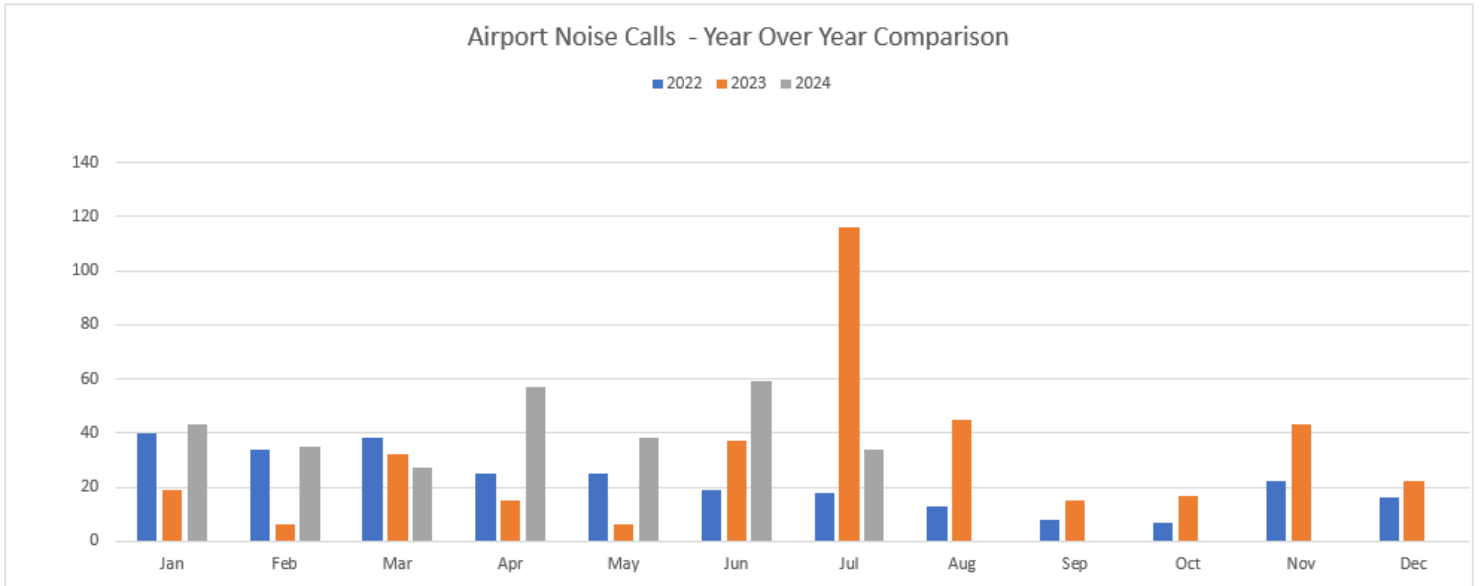
Noise Reports By Quadrant



NMT#1 Status: Installation completed and fully operational.

Airport Noise Calls: Year-Over-Year Comparison

	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec	Totals
2022	40	34	38	25	25	19	18	13	8	7	22	16	265
2023	19	6	32	15	6	37	116	45	15	17	43	22	373
2024	43	35	27	57	38	59	34						293



DATE: August 22, 2024
TO: Aviation Advisory Board
FROM: Rufus A. James, Airport Director
BY: Benjamin J. Gatti, Airport Operations Specialist
VIA: Carlton M. Harrison, Assistant Airport Director
SUBJECT: Development and Construction

12708 – FXE Runway 9 Runup Area Relocation and Southend Taxiways Project

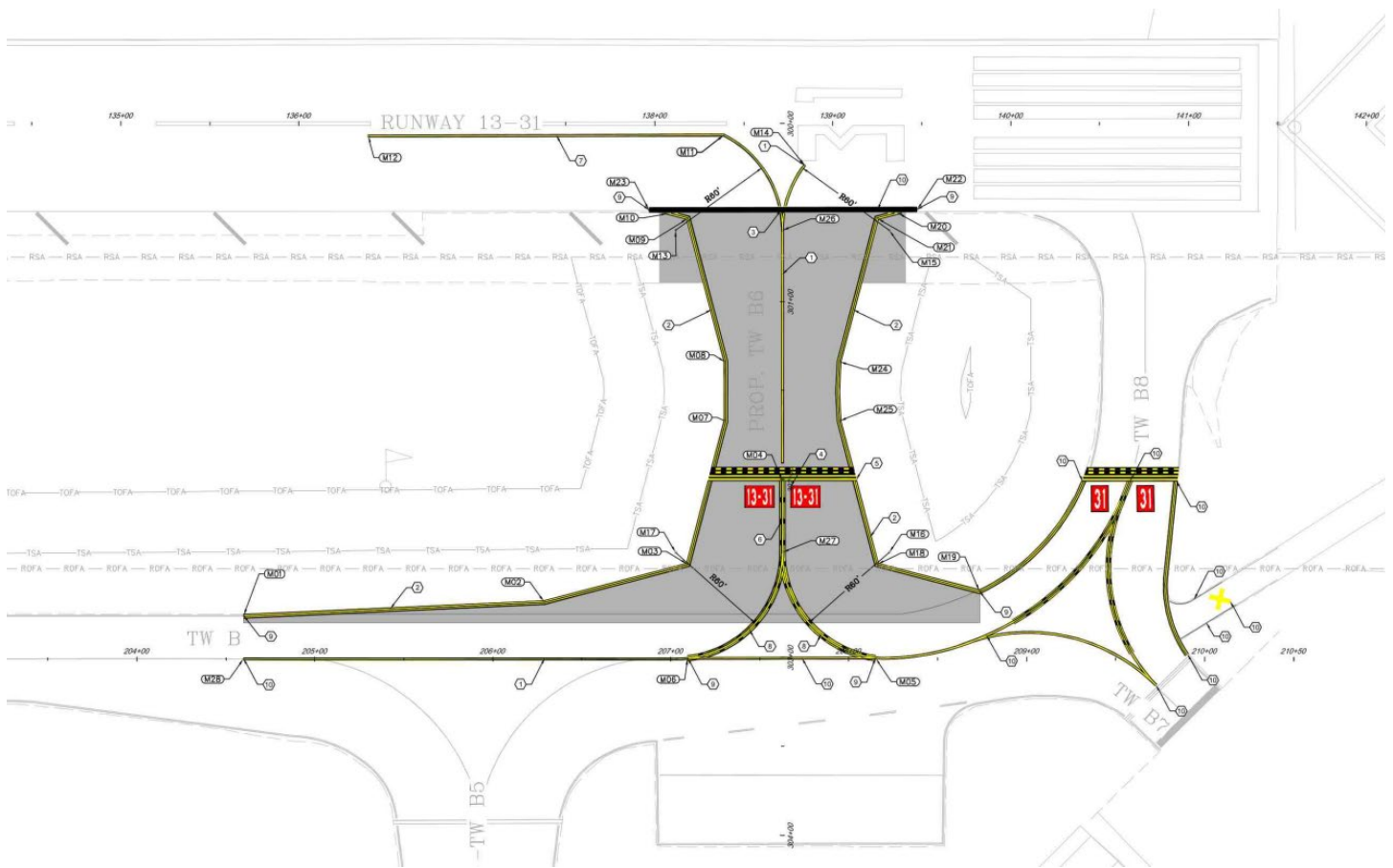
The purpose of the Runway 9 Runup Area Relocation and Southend Taxiways Project is to realign Taxiway Echo to adhere to modern FAA Advisory Circular standards for taxiways that intersect a runway. Construction began on December 11, 2023, with the closure of the Runway 9 Runup Area and the removal of the existing blast fence. Phases 5 and 6 of the project began on July 2, 2024, with the closure of Taxiway Echo between Taxiway Echo 2 and Taxiway Echo 5, including Taxiway Echo 3. This phase includes the milling and repaving of Taxiway Echo 3, Taxiway Echo between Taxiway Echo 3 and Taxiway Echo 5, and the construction of the new Runway 9 Runup Area. Phases 5 and 6 have an expected duration of approximately 60 days.

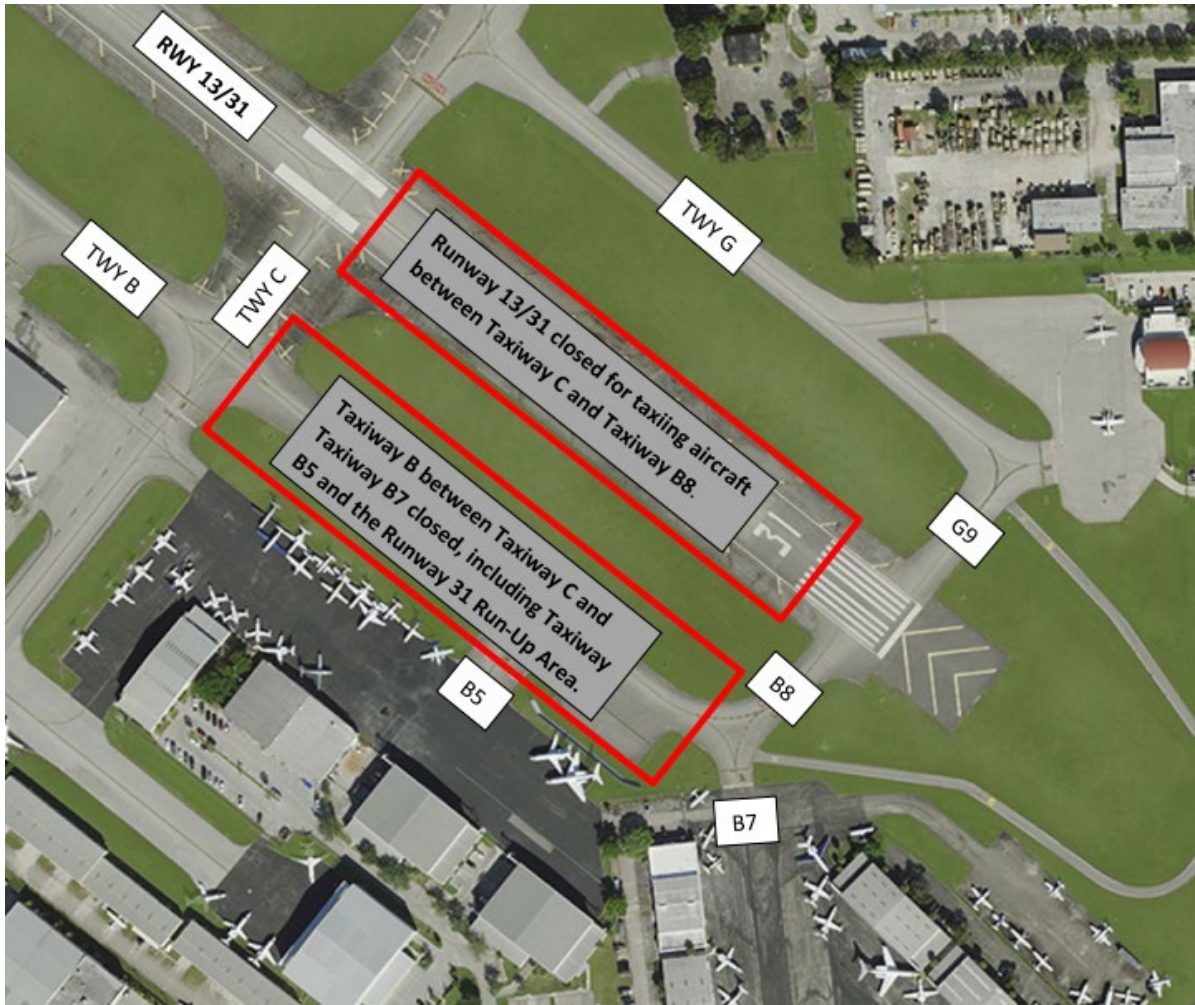




12521 – FXE Runway 31 Bypass Taxiway

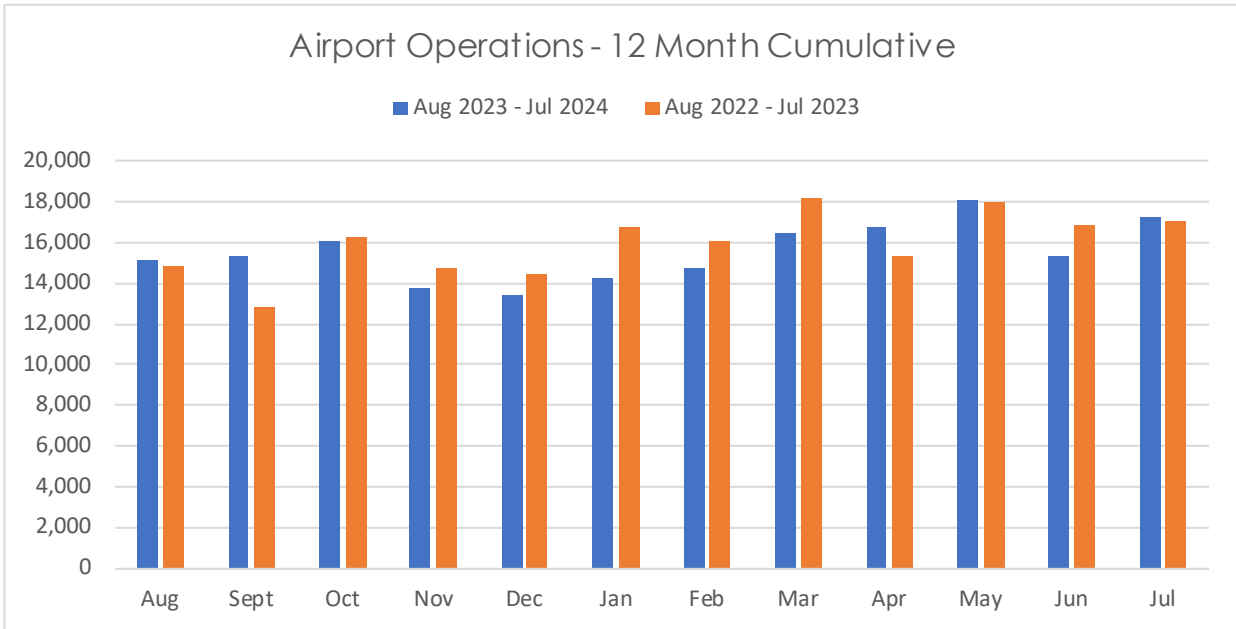
On April 8, 2024, Phase 1 of the Runway 31 Bypass Taxiway Project began. This project involves the installation of a new bypass taxiway connecting Taxiway Bravo to Runway 13/31 that adheres to modern taxiway geometry. The new taxiway will be installed between Taxiway Bravo 5 and Taxiway Bravo 8. The purpose of this project is to reduce congestion of Taxiway Bravo between Taxiway Charlie and Taxiway Bravo 8 when Runway 31 is in use. The project has reached substantial completion with the installation of asphalt pavement, new taxiway lights and taxiway signs, sod, and final paint markings. The contractor is working on punch list items, and the new taxiway is expected to be open by the end of August.





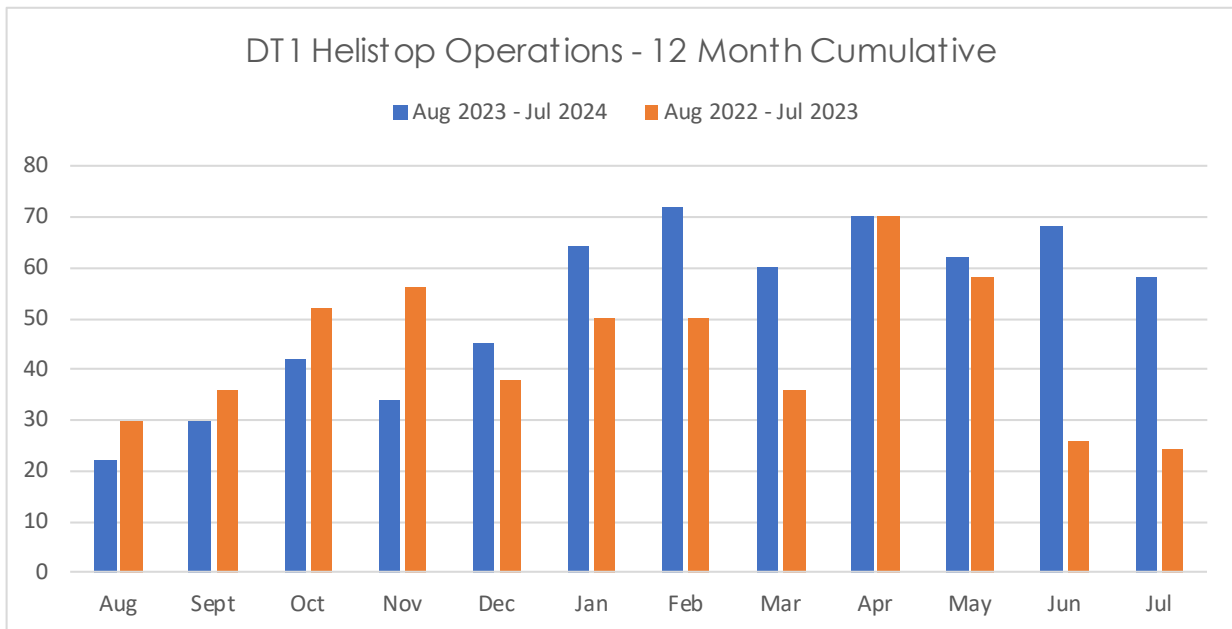
Airport Operations - 12 Month Cumulative

Aug 2023 - Jul 2024	15,145	15,294	16,020	13,692	13,431	14,231	14,721	16,444	16,787	18,089	15,357	17,236	186,447
Aug 2022 - Jul 2023	14,870	12,822	16,286	14,738	14,451	16,790	16,037	18,158	15,321	17,980	16,802	17,016	191,271



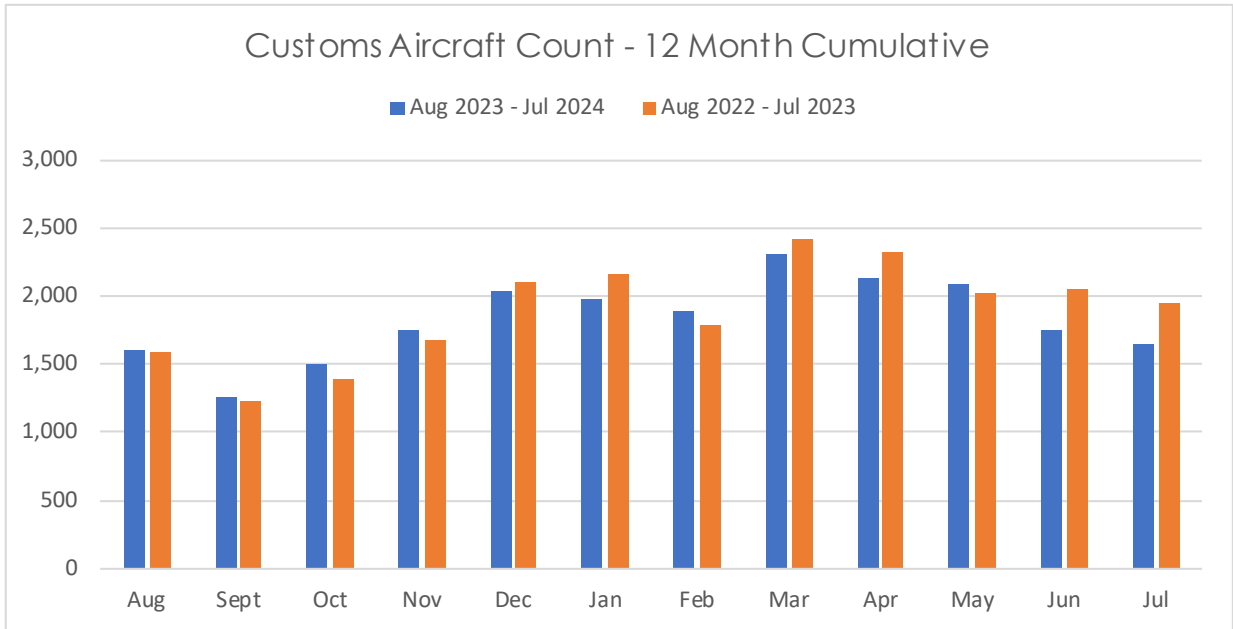
DT1 Helistop Operations - 12 Month Cumulative

Aug 2023 - Jul 2024	22	30	42	34	45	64	72	60	70	62	68	58	627
Aug 2022 - Jul 2023	30	36	52	56	38	50	50	36	70	58	26	24	526



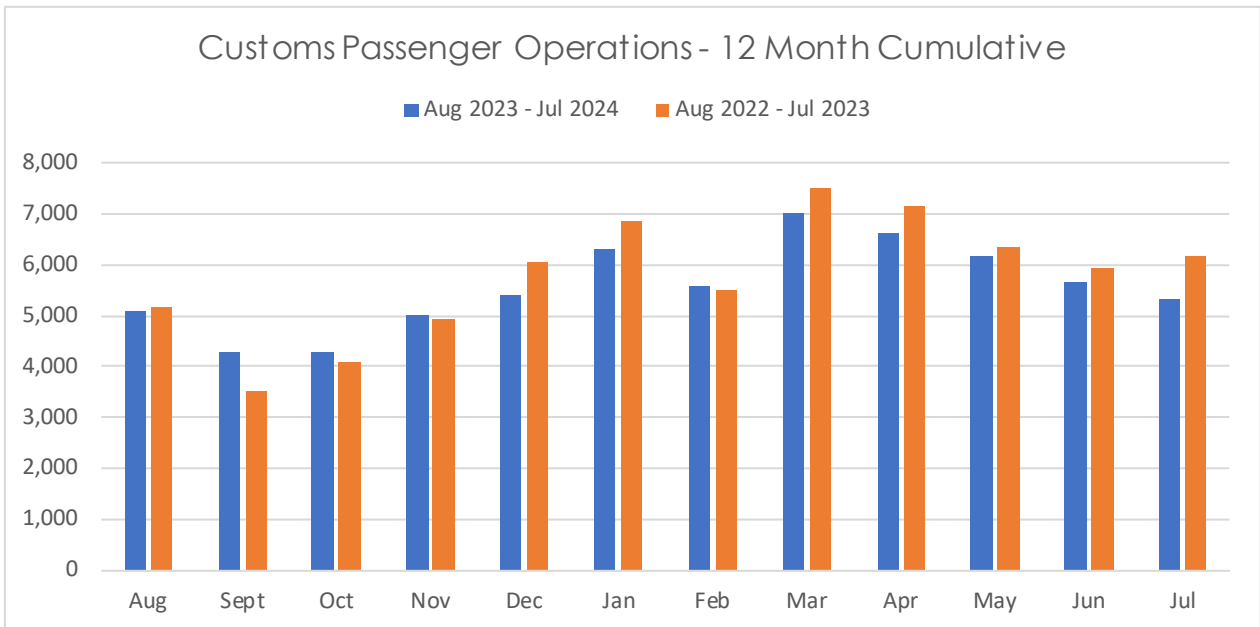
Customs Aircraft Count - 12 Month Cumulative

Aug 2023 - Jul 2024	1,603	1,257	1,488	1,749	2,041	1,984	1,887	2,306	2,123	2,080	1,747	1,644	21,909
Aug 2022 - Jul 2023	1,591	1,226	1,391	1,674	2,090	2,162	1,787	2,410	2,316	2,021	2,055	1,951	22,674



Customs Passenger Operations - 12 Month Cumulative

Aug 2023 - Jul 2024	5,070	4,284	4,299	5,016	5,420	6,300	5,561	7,023	6,600	6,160	5,649	5,335	66,717
Aug 2022 - Jul 2023	5,167	3,532	4,081	4,945	6,040	6,850	5,498	7,489	7,158	6,338	5,947	6,169	69,214

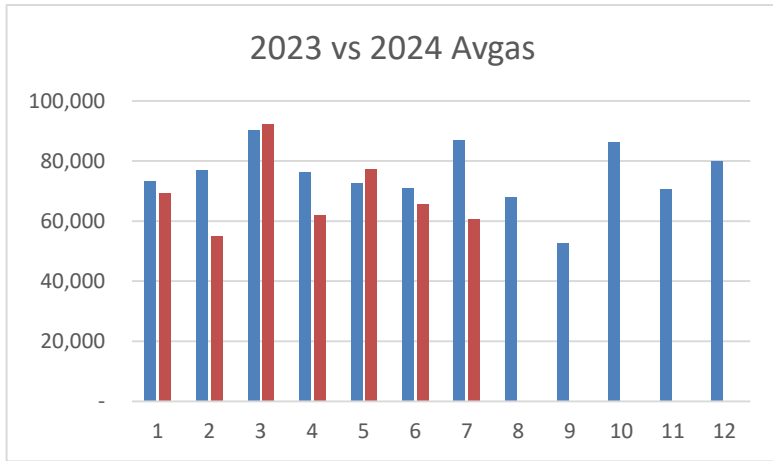


Date	Alert Level	Aircraft Type	Description
6/1/2024	II	Lear 35	Left Engine Out
6/10/2024	II	Piper Navajo	Unsafe Landing Gear Indicator
6/18/2024	I	Britten-Norman Islander	Flat Left Main Gear

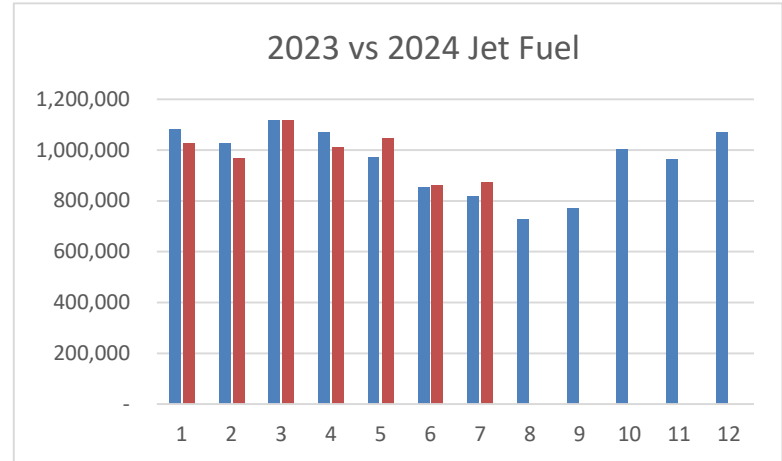
Date	Alert Level	Aircraft Type	Description
7/3/2024	I	Britten-Norman Islander	Flat Nose Gear
7/16/2024	II	Fairchild Metroliner	Left Main Gear Issue
7/22/2024	II	Mooney	Low Engine Power
7/26/2024	II	Piper Navajo	Low Oil Pressure Warning

2024 Monthly Fuel Flowage (Gallons)

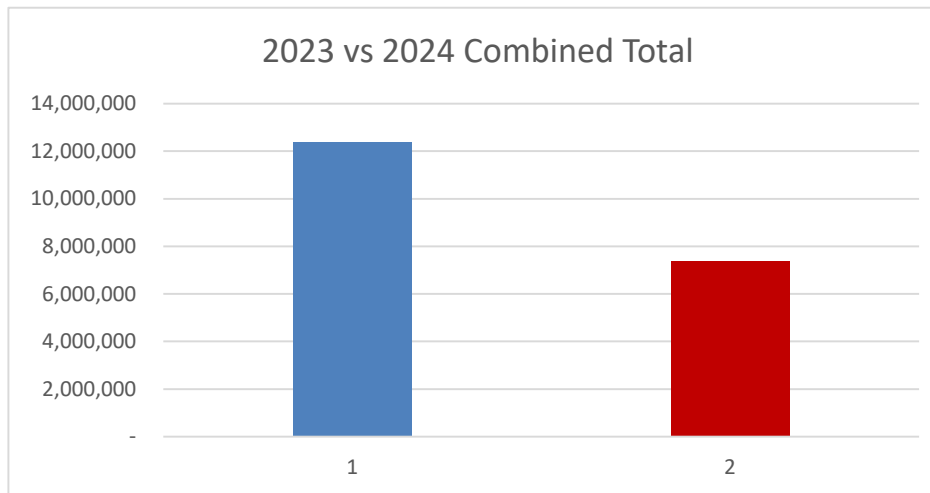
Year	Fuel	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec	Totals
2023	Avgas	73,336	77,070	90,175	76,298	72,673	71,179	86,936	67,957	52,499	86,295	70,605	79,918	904,941
	Jet	1,080,235	1,024,544	1,117,812	1,069,139	972,855	853,810	817,910	727,727	771,803	1,002,734	962,778	1,071,288	11,472,635
	Total	1,153,571	1,101,614	1,207,987	1,145,437	1,045,528	924,989	904,846	795,684	824,302	1,089,029	1,033,383	1,151,206	12,377,576
2024	Avgas	69,394	55,013	92,323	62,139	77,461	65,665	60,583						482,578
	Jet	1,026,986	968,551	1,115,643	1,011,625	1,046,671	859,942	873,318						6,902,736
	Total	1,096,380	1,023,564	1,207,966	1,073,764	1,124,132	925,607	933,901	0	0	0	0	0	7,385,314



■ 2023 ■ 2024



■ 2023 ■ 2024



■ 2023 ■ 2024

UPDATE ITEM C

DATE: August 22, 2024
TO: Aviation Advisory Board
FROM: Rufus A. James, Airport Director
SUBJECT: Arrearages



Rent

There are no arrearages to report.

Fuel Flowage

There are no arrearages to report.

Driven to Fly: A Car Dealer Transforms Into a CEO Business Aviator

July / August 2024

Half the week, Heather Cannon flies a Piper M500 to car auctions across the country, stocking her Ponca City, OK, dealership with the best-equipped vehicles.



Honda dealership President Heather Cannon leverages the power of business aviation to maximize long-distance travel time when sourcing inventory.

It was no longer working. Every Wednesday, Heather Cannon would catch an airline flight to Dallas, where she spent two days buying cars at auction and shipping them back to her dealership in Ponca City, OK. But more and more the flight was delayed or canceled, and she would miss the Wednesday auction.

“I started driving to Oklahoma City [with more airline connections], and if that flight was canceled, now I’ve wasted the hour-and-a-half drive each way, and I still didn’t get to the auction,” recalled Cannon, the president of Heather Cannon Honda. “I was sitting there, waiting for another delayed flight, and I thought, ‘I should just get my pilot’s license!’”

She was 26 years old, had worked in car dealerships since she was 15, and had no aviation background, no pilots in her family. It just seemed the best way to get from northern Oklahoma to major city auctions.

The average car dealership sells one used car for every new car. Most hire full-time buyers to find used vehicles, but Cannon doesn’t believe in that. “I want to be the one out there buying cars,” she said. Her keen eye and nose for a good deal have brought her success. “All the dealerships around me were buying cars in Tulsa, Oklahoma City and Wichita. The way I look at it, to be successful, you cannot play in the same sandbox as your competition.”

“... in the airplane, I can go to an auction in a remote town that would be a four-hour drive from a commercial airport.”

HEATHER CANNON, *President, Heather Cannon Honda*

Joining the Flight Community

In 2003, Cannon joined the Ponca City Aviation Booster Club. She also met her certified flight instructor (CFI), World War II pilot Ed Mang. She would be his last student before retiring. “He was real crusty, and tough with me, but that was just what I needed,” she said. “It took me to a whole other level.”

That year, she earned her license and bought a Cessna 182. She also met more flying mentors and started working on her instrument rating. “The pilots in town really welcomed me, they treated me like a daughter,” she said. “I’ve met so many great people through aviation.”

One of those pilot friends and mentors, Ed Jones, flew with her in the Skylane while she built hours, but Cannon still felt like a cautious aviator. In 2005, she traveled to Dallas-Fort Worth and completed her instrument rating in the Class Bravo airspace. “That’s where I finally gained some confidence,” she said.



Cannon credits her successful transition from a 182 to an M500 to a great mentor.

Cannon quickly progressed in her ratings, earning complex aircraft, seaplane, and multi-engine endorsements, and by 2006, her commercial license. That was also the last year Ponca City Regional saw any airline service.

Cannon was done driving to Oklahoma City. She was flying herself in the Skylane nearly every week from her hometown to car auctions in Dallas.

Hometown Hero

“Most weeks, I’m out buying cars,” said Cannon. “We’re always looking for inventory, and it takes more effort than just selling the cars you get as a trade-in. We’ve got to go find them, and in the airplane, I can go to an auction in a remote town that would be a four-hour drive from a commercial airport.”

Before an auction, every car up for sale is listed online, but Cannon does not rely on the condition reports. When she is on the ground, she will personally inspect over 100 cars a day. “The ability to walk those cars, smell them – that’s important,” she said. “I can see something that wasn’t in the report. That’s what differentiates me from buying online.”

The days she is away, Cannon can trust the dealership will operate just the same. “I have a fantastic team, we all have a lot of longevity here,” she said.

“I’ll land in a city, walk cars for five or six hours, fly to the next auction, and do it again. Many times, I’ll hit three different states in one week.”

HEATHER CANNON, *President, Heather Cannon Honda*

In high school, Cannon worked as an accounts receivable clerk for one of the half-dozen car dealerships in town. She was raised by a single mother and worked six hours a day after school.

After earning her pilot’s license, she thought she’d pursue a career as an air traffic controller, and went to a two-year aviation program in Miami. She finished in 16 months and was hired by the FAA, but then a management opportunity opened up at another car dealership in Ponca City.

She started at the bottom, and now her name is on the building. “It was a lot of 80-hour weeks for many years, but it’s finally paid off,” Cannon said. “I’m glad I ended up in the car business.”

'I'm Going to Fly It Myself'

In the 2000s, when Cannon was working her way up, the internet was transforming the car business. Cars were harder to get by just flying in the 182 to auctions in Dallas. She started flying to Dallas on Tuesday, to Alabama on Wednesday, Tennessee on Thursday, and then maybe a car auction in New York before returning home to Ponca City.

"I'll land in a city, walk cars for five or six hours, fly to the next auction, and do it again," she said. "Many times, I'll hit three different states in one week."

By 2021, she was flying with another mentor in the right seat of the 182, Marvin Janda, a pilot retired from a local oil company. They both knew Cannon needed more range. As she looked to move up, Janda listed every model to consider. She ended up buying a Piper M500.

Janda went to Legacy Flight Training in Scottsdale, AZ, to do his initial on the M500. He was now flying in the left seat, but he got his CFI certificate back, so every hour Cannon flew with him in the new airplane went in her logbook.

"That was a big deal coming straight from a 182 to an M500," she said. "It's virtually impossible to do, but it can happen if you've got a great mentor."

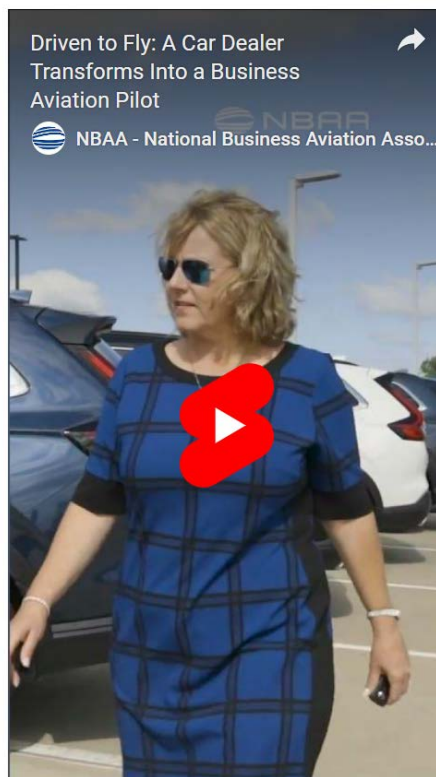
HEATHER CANNON, *President, Heather Cannon Honda*

"To sit up front, the insurance required me to go to Legacy too, so I did," she said. "Then, in less than a year, Marvin took another opportunity. I said, either I'm going to fly it myself, or I've got to sell it."

After 130 hours, the insurance company signed off on Cannon as a pilot-in-command of the M500. "That was a big deal coming straight from a 182 to an M500," she said. "It's virtually impossible to do, but it can happen if you've got a great mentor."



During business trips, Cannon relies on her trusted colleagues to run the dealership. "I have a fantastic team."



Looking at the Whole U.S.

With the speed and range of the M500, Cannon estimates owning her own aircraft saves her 60 hours a month in travel time.

Combined with the freedom of flying herself she can take advantage of regional differences in car prices to bring the most in-demand vehicles back to her dealership at a discount.

“Once I acquired the M500, I spreadsheeted the entire country for what car auctions are out there. You can look at the whole U.S.,” she explained. “You can see that a Chevy Suburban is selling for less in California, or maybe Detroit, and you have the option in this airplane, it doesn’t matter where you go, it’s five hours anywhere.”

Cannon can offer the most sought-after vehicles at such competitive prices that car buyers often come to Heather Cannon Honda from as far as Arkansas or Missouri. “I’ll sell as many cars as an Oklahoma City dealer,” she said. “Last week, we had a guy fly in from Virginia.”

[Learn more about Cannon’s business at cannonhonda.com.](https://cannonhonda.com)

Fits 'Like a Glove'

Heather Cannon likes to do recurrent training every six months at minimum. “When I get in the airplane, I want it to fit me like a glove,” said the president of Heather Cannon Honda. “I know that every time I train with someone, I always learn something.”

When she started flying the M500 in 2021, she immediately joined the Piper M-Class Owners & Pilots Association (PMOPA), where she met the maintenance technician who does her annual inspections and several instructors widely known for their flight training.

Although she lives in flat Ponca City, OK, she completed mountain training with one of those Piper M-Class instructors. She has also done spin-and-stall training. “I’ll just be honest: I hate it,” she said. “I get sick. I don’t like it at all, but I knew I needed it – because of the confidence it gives you.

“I like to train with different people in different scenarios,” Cannon said. “Every time, I learn a little nugget.” She cites many mentors – and she is a mentor herself, conducting over 100 Young Eagle flights.

That’s what aviation is to Cannon. “I’ve had a lot of great mentors along the way,” she said, “and it took every one of them to get me where I am today.

[Find out more about the Piper M-Class Owners & Pilots Association \(PMOPA\) at pmopa.com.](https://pmopa.com)



Honda dealership President Heather Cannon leverages the power of business aviation to maximize long distance travel time when sourcing inventory.

Snapshot

Aircraft: One Piper M500

Base: Based at Oklahoma's Ponca City Regional Airport (PNC)

Personnel: Heather Cannon is the president and owner/operator.